

The Unified Government of Athens-Clarke County TSPLOST Programs 2018 & 2023

Oversight Committee Meeting Agenda

January 27, 2025 @ 5:15 p.m. In-Person at the Planning Department Auditorium
Public Viewing of Meeting at: <https://youtube.com/live/RsxIWTTt-lbU?feature=share>

NOTE: The Oversight Committee meeting is open to the public. However, public comments are not received at the committee meeting. All reports are draft.

The full agenda and attachments can be found at:

<https://www.accgov.com/Calendar.aspx?EID=27152&month=1&year=2025&day=6&calType=0>

1. Welcome / Comments – Welcome / Roll Call:

Frank Platt (Chair) Jermal Chambers Jen Calabria Andrea Jolliffe Bob Bonsall Allen Jones
Daun Fest Daryl McManus Jeremy Field

Staff:
Josh Hawkins, Capital Projects Director
Keith Sanders, SPLOST Program Administrator
Rani Katreeb, Transportation & Public Works Assistant Director
Liz Mann, SPLOST Program Support Specialist

2. Meeting Minutes Review and Approval

- December 16, 2024
- November 18, 2024
- October 21, 2024

3. Committee Actions:

- TSPLOST 2023 Project 21, CY25 Pavement Maintenance Program - Roadway List & Project Resolution

4. Review of TSPLOST Program Monthly Reports

- TSPLOST 2018 Monthly Project Update
- TSPLOST 2023 Monthly Project Update
- TSPLOST 2018 Program Revenue Report
- TSPLOST 2018 Program Expenditure Report
- TSPLOST 2023 Program Revenue Report
- TSPLOST 2023 Program Expenditure Report

5. Other Business

6. Next Meeting Date – Tentatively Monday, February 17, 2025 - 5:15 P.M.

7. Adjournment

Attachments:

- Action Item(s) as listed above
- TSPLOST 2018 Monthly Project Update
- TSPLOST 2023 Monthly Project Update
- TSPLOST 2018 Program Revenue Report
- TSPLOST 2018 Program Expenditure Report
- TSPLOST 2023 Program Revenue Report
- TSPLOST 2023 Program Expenditure Report



Draft - Meeting Minutes
ATHENS-CLARKE COUNTY
TSPLOST 2018 and 2023 OVERSIGHT COMMITTEE
December 16, 2024 @ 5:15 P.M.
Planning Department Auditorium
In-Person w/Live Stream Meeting
<https://youtube.com/live/1CSCRrDSa5g?feature=share>

MEMBERS PRESENT: Frank Platt (Chair), Allen Jones, Bob Bonsall, Daryl McManus (Virtual)

MEMBERS ABSENT: Jeremy Field, Daun Fest, Andrea Jolliffe, Jermal Chambers, Jen Calabria,

STAFF PRESENT:

Josh Hawkins, Capital Projects Director
Keith Sanders, SPLOST Program Administrator
Mike Kajder, SPLOST Project Administrator
Melissa Byler, SPLOST Program Support Specialist

GUESTS: None

GENERAL BUSINESS

Frank Platt Chair suspended meeting proceedings at 5:25 p.m., due to lack of present quorum.

COMMITTEE ACTION ITEMS

- TSPLOST 2023 Project 15, Sycamore Drive Pedestrian Improvements Project, Proposed Project Concept

OTHER BUSINESS

The next meeting is tentatively scheduled for Monday, January 27, 2024, at 5:15 P.M. in person in the Planning Department Auditorium.

The above summation is an interpretation of the items discussed and decisions reached at the above referenced meeting, not a transcript of the meeting. A digital recording of the meeting is available upon request. Anyone desiring to add to, or otherwise correct the minutes, is requested to return written comments to the Capital Project Director by the date of the next meeting.

Keith Sanders
SPLOST Project Administrator

A handwritten signature of Keith D. Sanders.

Attachments: NONE



Draft - Meeting Minutes
ATHENS-CLARKE COUNTY
TSPLOST 2018 and 2023 OVERSIGHT COMMITTEE
November 18, 2024 @ 5:15 P.M.
Planning Department Auditorium

MEMBERS PRESENT: Frank Platt (Chair), Allen Jones, Bob Bonsall

MEMBERS ABSENT: Jeremy Field, Daun Fest, Andrea Jolliffe, Jermal Chambers, Jen Calabria, Daryl McManus

STAFF PRESENT:

Keith Sanders, SPLOST Program Administrator
Mike Kajder, SPLOST Project Administrator
Liz Mann, SPLOST Program Support Specialist

GUESTS: None

GENERAL BUSINESS

Frank Platt Chair suspended meeting proceedings at 5:23 p.m., due to lack of present quorum.

COMMITTEE ACTION ITEMS

- TSPLOST 2023 Project 15, Sycamore Drive Pedestrian Improvements Project, Proposed Project Concept

OTHER BUSINESS

The next meeting is tentatively scheduled for Monday, December 16, 2024, at 5:15 P.M. in person in the Planning Department Auditorium.

The above summation is an interpretation of the items discussed and decisions reached at the above referenced meeting, not a transcript of the meeting. A digital recording of the meeting is available upon request. Anyone desiring to add to, or otherwise correct the minutes, is requested to return written comments to the Capital Project Director by the date of the next meeting.

Keith Sanders
SPLOST Project Administrator

A handwritten signature in black ink that reads "Keith D. Sanders".

Attachments: NONE



Draft - Meeting Minutes
ATHENS-CLARKE COUNTY
TSPL0ST 2018 and 2023 OVERSIGHT COMMITTEE
October 21, 2024 @ 5:15 P.M.
Planning Department Auditorium

MEMBERS PRESENT: Frank Platt (Chair), Allen Jones, Jermal Chambers, Jen Calabria, Daryl McManus, , Jeremy Field, Bob Bonsall, Jeremy Field (Remote)

MEMBERS ABSENT: Daun Fest, Andrea Jolliffe

STAFF PRESENT:

Josh Hawkins, Capital Projects Director
Keith Sanders, SPLOST Project Administrator
Katreeb, Assistant Director Transportation & Public Works
Liz Mann, SPLOST Program Support Specialist
Melissa Byler, SPLOST Program Support Specialist (remote)

GUESTS: None

GENERAL BUSINESS

Frank Platt Chair called the meeting to order at 5:17 p.m., welcomed the members in attendance, and thanked them for being present. Frank Platt called roll and marked members in attendance.

MEETING MINUTES REVIEW AND APPROVAL

Jen Calabria made a motion to approve corrected minutes dated **September 16, 2024**. Daryl McManus seconded that motion. The motion carried and was approved unanimously.

COMMITTEE ACTION ITEMS

- TSPL0ST 2023 Project 04, Atlanta Highway / W. Broad Street Improvements, Project Concept and Authorization to Partner with GDOT on West Broad Street Road Safety Project with Multi-use Path, from Epps Bridge Road to Hodgson Dr.

Discussion: Rani Katreeb reviewed the partnership with GDOT on the project and how the work will be distributed. Rani Katreeb reviewed the Project Concept. The Oversight Committee discussed the Proposed Project Concept and the partnership with GDOT.

Motion made by Allen Jones to confirm that the proposed Project Concept, as presented, is consistent with the Initial Project Statement for TSPL0ST 2023 Project 04, Atlanta Highway / W. Broad Street Improvements.

Seconded by Bob Bonsall. The motion carried unanimously.

MONTHLY PROGRAM REPORTS

TSPLost 2023 Oversight Committee Meeting Minutes October 21, 2024

Page 2

Keith Sanders reviewed the monthly reports for the TSPLost 2018 and 2023 programs.

OTHER BUSINESS

The next meeting is tentatively scheduled for Monday, November 18, 2024, at 5:15 P.M. in person in the Planning Department Auditorium.

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Keith Sanders
SPLOST Project Administrator



Attachments: NONE

**THE UNIFIED GOVERNMENT OF ATHENS-CLARKE COUNTY
COMMISSION AGENDA ITEM**

SUBJECT: TSPLOST 2023 Project 21, CY25 Pavement Maintenance Program - Roadway List & Project Resolution

DATE: January 06, 2025

EXECUTIVE SUMMARY:

The proposed requested action will confirm that the Roadway List & Project Resolution for TSPLOST 2023 Project 21, Pavement and Bridge Maintenance Program, as generally shown in **Attachment #1**, is consistent with the Initial Project Statement for TSPLOST 2023 Project 21, Pavement and Bridge Maintenance Program.

BUDGET INFORMATION:

REVENUES:	\$4,668,633	FY24-25 General Fund Capital Budget
	\$20,351,000	TSPLOST 2023 Project #21 (Paving Portion, all tiers)
	\$1,555,750	2025 GDOT LMIG Program
	<hr/>	
	\$26,575,383	Total Funds Available

EXPENSES:	\$12,483,620	Current Expenses & Encumbrances (Previous Programs)
	\$ 122,345	Paving Inspection Staff Salary & Benefits (Designated Funds)
	\$ 75,000	Materials Testing and Inspection Services (Designated Funds)
	\$ 5,437,750	Future Program Funds (Designated Funds)
	\$ 1,000,000	Future Epps Bridge Pkwy Preventative Maintenance (Designated Funds)
	<hr/>	
	\$ 344,485	Program Overall Contingency (Designated Funds)
	<hr/>	
	\$ 7,112,183	Available for Construction of CY25 Pavement Management Program Project (including \$300,000 Extra Work Contingency Allowance)

FUNDING SOURCE: General Fund Capital Budget
TSPLOST 2023 Project 21 (paving portion)
GDOT Local Maintenance and Improvement Grant (LMIG)

COMMITTEE ACTION REQUESTED ON: January 27, 2025

PURPOSE & REQUESTED COMMITTEE ACTION:

To request that the TSPLOST 2023 Oversight Committee:

- a. Confirm that the Roadway List & Project Resolution for TSPLOST 2023 Project 21, Pavement and Bridge Maintenance Program, as generally shown in **Attachment #1**, is

consistent with the Initial Project Statement for TSPLOST 2023 Project 21, Pavement and Bridge Maintenance Program.

OR

- b. Deny that the Roadway List & Project Resolution for TSPLOST 2023 Project 21, Pavement and Bridge Maintenance Program, as generally shown in **Attachment #1**, is consistent with the Initial Project Statement for TSPLOST 2023 Project 21, Pavement and Bridge Maintenance Program.

OR

- c. Committee defined option

HISTORY:

1. In 1997, Transportation and Public Works Department (T&PW) staff implemented a comprehensive Pavement Management Program (PMP) to address maintenance needs of the public roadway system. T&PW staff evaluates and updates the program on an annual basis.
2. On November 7, 2017, the voters of Athens-Clarke County approved the TSPLOST 2018 Program, which included funding for Pavement Rehabilitation.
3. On December 5, 2017, M&C approved the initial TSPLOST 2018 Program Implementation Schedule identifying the annual PMP funding from TSPLOST 2018 Program #07 to be used in conjunction with other fund sources.
4. On March 6, 2018, M&C approved the first calendar year PMP Project list for the CY18 PMP to match the paving season, which typically starts in March and ends in November. Since 2018, paving projects have been grouped into calendar year (CY) rather than fiscal year (FY).
5. On March 27, 2019, T&PW issued a Notice to Proceed to Arcadis, U.S., to perform automated pavement condition assessment and inventory of all ACCGov roadways including inventory of signs and intersections utilizing an elementary pavement management software, Micro Paver (Paver™). This was the first automated rating performed on locally maintained roadways.
6. In April 2019, T&PW and other departments began utilizing Cityworks, a cloud-based work order and asset management system, to assign and track day to day operations for staff and to create an asset inventory.
7. On June 15, 2020, Manager Williams shared with the M&C an Interdepartmental Memorandum (IDM) with updates from T&PW on the new automated pavement rating initiative, including history and methodology used to assess the condition of roadway pavement. The memo describes the old rating system process in use through CY20 and the new pavement rating system to be implemented in CY21 and into the future.

8. On May 24, 2022, voters of Athens-Clarke County passed the TSPILOT 2023 referendum, which included Project No. 21, Pavement Rehabilitation.
9. On June 7, 2022, M&C approved the selection of 6 firms to provide on-call engineering services, one of which is KCI Technologies, Inc. The recommendations for these 6 firms were based on technical ability, experience, resources, references, interviews and overhead & profit/cost multipliers.
10. On June 10, 2022, under suspension of rules, M&C approved the CY22 PMP list of roadways and associated maintenance activities recommended by staff, approved the designation of Pittman Construction Company as the apparent low responsive and responsible bidder for the CY22 PMP project, awarded a Unit Price Construction Services Contract in the amount of \$8,149,423.70 (three-year term renewable contract), and authorized the Mayor to execute all related project and contract documents.
11. On January 26, 2023, KCI Technologies, Inc. provided T&PW staff with a scope and fee proposal titled “Pavement Management Services and Right-of-Way Assets Inventory and Condition Assessment” including:
 - a. Core Task: pavement inventory and condition assessment with multiyear maintenance software training compatible with the ACCGov Geographical Information System (GIS) and City Works System for a cost not to exceed \$332,900; and
 - b. Additional Task: right-of-way infrastructure inventory and assessment including traffic signs, pavement markings, pavement striping, curb & gutter, sidewalk curb ramp, and traffic light inventory and condition assessment for a total fee not to exceed \$464,100 (available within 12 months from notice to proceed on KCI’s Core Task).
12. On March 7, 2023, M&C approved a contract for services through KCI Technologies to provide a comprehensive pavement inventory and condition assessment with multiyear maintenance report and software training for a cost not to exceed \$332,900.
13. On July 19, 2023, Change Order #1 was executed to existing KCI Technologies Task Order to develop a curb and gutter inventory and curb reveal elevation and to perform a Ground Penetrating Radar (GPR) evaluation of selected streets owned by ACCGov and located in Athens-Clarke County, for a cost not to exceed \$45,750.
14. On February 13, 2024 in a Work Session meeting, T&PW staff and KCI consultant presented KCI pavement rating results for all publicly maintained roadways and solicited feedback on funding, future treatment strategy and priority.
15. On March 21, 2024, T&PW staff received a draft 2023 Pavement Evaluation & Management Plan report from KCI consultant. T&PW staff received final report in July 2024

(see History #18).

16. On June 5, 2024, M&C approved the list of roadways and associated maintenance activities for the CY24 PMP project and the one-year extension to the construction contract with Pittman Construction Company, Inc. for the CY24 PMP project at a cost not to exceed \$8,415,454.56. This was the second and last renewal option to the CY22 PMP Project Contract.
17. On June 28, 2024, GDOT announced the LMIG funds available for Fiscal Year 2025. Following the formula used by GDOT, which is based on lane miles of roadway and population, the Unified Government of Athens-Clarke County (ACCGov) is eligible to receive \$1,555,749.97 as part of the 2025 LMIG Program. The deadline to submit the grant application to GDOT is February 1, 2025.
18. On July 31, 2024, T&PW staff received KCI's final Pavement Evaluation & Management Plan report that including Curb & Gutter data base (Facts & Issues No. 8 & 9, and **Attachment #3**).
19. On January 2, 2025, M&C authorized T&PW staff to submit, and accept, if awarded, the 2025 GDOT LMIG application in the amount of \$1,555,749.97 and adopted associated budget ordinance. It is expected that 2025 GDOT LMIG awards will be announced in mid to late February 2025.
20. On January 9, 2025, T&PW and Finance staff began the advertisement and solicitation of sealed bids on the proposed CY25 PMP Project, as shown on **Attachment #1**. Opening of bids is currently scheduled for February 13, 2024 and is subject to change per future addendums.

FACTS & ISSUES:

1. The Initial Project Statement for **TSPL0ST 2023 Project 21 Pavement and Bridge Maintenance Program** reads as follows:

Project 21 – This program will provide continuation of preventive road and bridge maintenance, road reconstruction, and/or rehabilitation activities for various streets and roadways throughout ACC. Depending upon actual costs and funding availability, improvements may include, but are not limited to: crack sealing, full depth patching, resurfacing, roadway reconstruction with the intent to preserve and/or extend the life of existing roadways, in addition to bridge maintenance, bridge improvements, bridge repairs, and/or bridge replacements. This is a continuation of the SPLOST 2005, SPLOST 2011, and TSPL0ST 2018 Program funding. To the extent allowed by law, budgeted project funds may be used as matching funds for leveraging grant opportunities. Additionally, to the extent allowed by law, other funding sources, including, but not limited to, grants, may be utilized to offset and/or reduce the project budget. Any unspent project funds that

accrue as a result of the receipt of grants or other funding sources may be assigned to other approved TSPLIST projects.

2. ACCGov owns and maintains 628 centerline miles (1270 lane-miles) of public roadways including 81.27 centerline miles of collectors, 85.20 centerline miles of arterials, and 461.46 centerline miles of local streets.
3. Pavement maintenance activities require temperatures of at least 45 degrees Fahrenheit, and therefore typically take place between the months of March and October. To match the paving season, T&PW staff began to group the delivery of paving activities into calendar year (CY) rather than the fiscal year (FY).
4. T&PW requested PMP funding from the 2023 TSPLIST Program of \$75,000,000 to address the accumulated deficit roadways with a Pavement Condition Index (PCI) score ≤ 40 as well as future pavement maintenance needs.
5. The 2023 TSPLIST was approved with \$22,000,000 funding for the Pavement & Bridge Maintenance Program, including \$1,649,000 designated for Bridge Maintenance and \$20,351,000 for PMP projects. Funding for the PMP also includes General Capital Funds, SPLOST, and GDOT LMIG Funds.
6. In calendar year 2020 (CY20), T&PW initiated an improved PMP methodology that utilized 2019 automated condition ratings (PCI) to inform future PMP Projects, and it consisted of the following elements:
 - a. Inventory and evaluation of ACCGov roadways: A consultant performs an automated pavement condition and distress data collection utilizing advanced and sophisticated equipment mounted on a vehicle. A condition assessment and numerical Pavement Condition Index (PCI) score is generated for each roadway segment based on industry standards and the American Societies for Testing and Materials (ASTM) Standard D6433. Automated rating and surveying will be conducted every four years on all ACCGov maintained public roadways.
 - b. Asset maintenance software & tools: PCI scores are entered into PAVER™ (also known as MicroPaver) software, a Pavement Maintenance Management System originally developed in the late 1970s for the Department of Defense (DOD) to manage its vast inventory of pavements. The PCI for roads and parking lots became an ASTM standard in 1999 (D6433-09). PCI scores are entered into the software every four years concurrent with future ratings.
 - c. Pavement maintenance activities selection: T&PW staff work with consultants and local contractors to set up the PAVER™ software with preferred pavement treatments and historic unit costs.
 - d. Annual project development: T&PW staff use the PAVER™ software to generate a multi-year program with alternative treatments based on the PCI based on available budget and preferred average PCI scoring.

7. Between 2019 and 2023, T&PW staff utilized PAVER™ programing order to maximize efficiency and transparency providing data driven approach to annual paving programs. The PAVER™ program did not meet all expectations, triggering a new initiative for CY24 PMP Project List to utilize City Works, an already acquired asset management software, followed by the purchase of Pavement Express application offered as an extension to City Works (History #6). City Works software is already in use by multiple ACCGov departments for day-to-day work orders.
8. Newly collected roadway PCI scores, in 2023, by on-call consultant KCI (History #15 & 18) are utilized to develop annual paving programs, starting with CY24 PMP Project List (resurfacing & preventative maintenance lists), based on available funding. The table below explains the scores and preferred applicable treatment in a perfect funding scenario:

PCI Range	Roadway Condition	Preferred Treatment (goal)
85-100	Excellent	Like new conditions – crack sealing planned four years from resurfacing
70-85	Very Good	Good to very good conditions that may\should receive crack sealing as preventative treatment
55-75	Good	Good conditions that should have received crack sealing and may be considered for other preventative treatments based on observed localized deteriorations.
40-55	Marginal	Acceptable conditions that require immediate planning for heavier surface treatments, thin overlays and\or resurfacing. May require immediate localized treatments.
25-40	Poor	Not acceptable condition that Requires overlays, surface replacement, base reconstruction, and\or possible subgrade stabilization.
0-25	Very Poor	Similar to poor conditions with high percentage of full reconstruction.

9. KCI's 2023 Pavement Evaluation and Management Report is now complete, and it provides a complete picture of ACCGov's roadway pavement ratings and future needs based on a desirable average PCI, see figure 22 below (see **Attachment #3** for complete report):
10. Based on observed pavement deterioration, T&PW's goal and industry standard set a

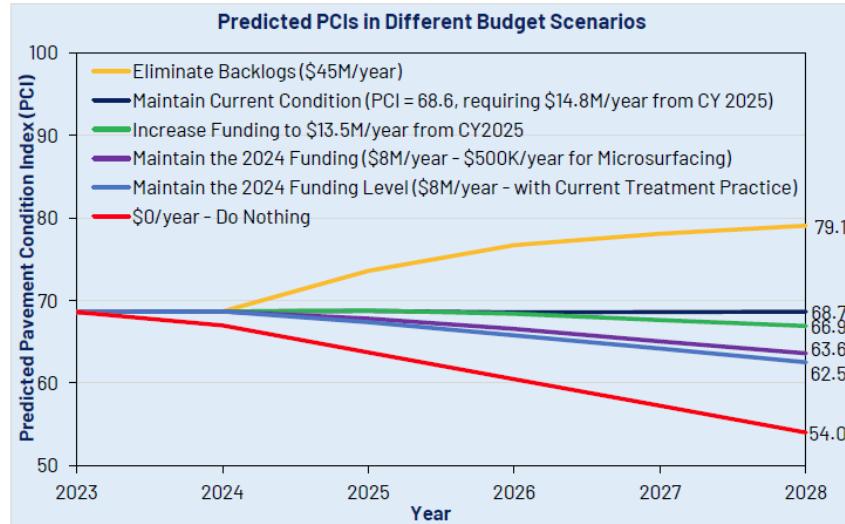


Figure 22. Effect of funding level on County's pavement conditions.

resurfacing frequency of 10 years for arterials, 15 years for collectors, and 20 years for local roads. This equates to resurfacing 75 lane-miles annually for a weighted frequency of about 17 years as opposed to the current 50-year cycle which is based on current funding. T&PW staff agrees with KCI's report and recommends that the annual PMP funding would need to be approximately \$13.4 million to achieve the recommended frequency, but this level of funding still does not address the backlog in paving that has accumulated from years of underfunding.

11. Pavement deterioration rates are different for each roadway. Therefore, PCI scores change over time and, when asset management tools are utilized over a number of years (10 to 20 years), it will allow departmental staff to better predict needed maintenance including resurfacing in the future. Proactive planning, periodic inspections, preventative maintenance, and adequate funding, will better equip T&PW staff to accurately provide future paving and maintenance schedules.

12. Roadway surface deterioration is driven by multiple factors including:
 - a. Traffic loading: this includes total annual traffic counts and the type of vehicles navigating the roadway.
 - b. Roadway construction and subsurface integrity: This can vary based on the roadway pavement thickness and whether if there is adequate sub-base or not. For example, roadways built without graded aggregate stone are prone to swelling and deterioration faster than roads that have the adequate subsurface construction.
 - c. Utility cuts: All utility cuts are prone to failure, and the lack of oversight and quality inspections may lead to substandard work that may fail and cause water to seep into cracks and cause major pavement failures.

- d. Weather: Water and excessive low and high temperature will contribute greatly to escalating deterioration rates. With contraction and expansion comes more cracks that allow for more water to enter into the roadway subsurface that leads to roadway surface and subsurface failure.
- e. Lack of funding: Minimal preventative maintenance such as crack sealing, thin patching, and thin overlay will cause the pavement surface to deteriorate faster. It becomes far more expensive to rehabilitate and more likely to require extensive preparatory work prior to resurfacing.

13. The CY24 PMP Project contractor, Pittman Construction, has completed 60% of the CY24 PMP Project. T&PW staff continues to work with the contractor on expediting progress to meet substantial completion date of January 31, 2025. The contractor will have 30 days, from Substantial Completion date, to complete all corrective items.

14. Typically, PCI rating is given to a short segment of the roadway, reflecting a granular look and small portion of the entire stretch of one roadway. For example, one roadway that is typically paved all at once may have multiple segments, and each segment has its own rating (PCI). Facts & Issues No. 15 through 17 explain how the CY25 PMP Project List was selected.

15. The selection matrix developed by KCI will first examine segments with low PCI ($PCI \leq 55$) and determine the best approach/treatment to improve the roadway condition. If there is a localized deterioration present that can be addressed via limited deep patching treatment, the road segment is placed in the preventative maintenance list for deep patching. Otherwise, the road and adjoining segments will be examined under the resurfacing treatment matrix (F&I #16 & 17). Additional roads with abnormal localized deterioration that exceeds TPW's in-house ability to repair are usually provided by Street & Drainage Division staff and added to the recommended deep patching list.

16. There are three GDOT roadway classifications. It is T&PW staff and KCI recommendations to set a different PCI threshold for resurfacing and/or rehabilitation per classifications as follows:

- a. Local – $PCI \leq 40$
 - i. These roadways provide access to homes, businesses, and other property
 - ii. Posted speed generally 20-25 mph (may be greater)
 - iii. Have a low truck, heavy equipment, and non-commercial traffic volumes
- b. Collector – $PCI \leq 48$
 - i. These roadways link arterials and local roads and perform some of the duties of each.
 - ii. Posted speed typically exceeds 30 MPH
 - iii. May have residents - rural sections
 - iv. Have higher truck and heavy equipment traffic volumes than local roadways.
- c. Arterials – $PCI \leq 55$

- i. These roadways provide mobility so traffic can move from one place to another quickly and safely.
 - ii. Posted speed 40+ MPH
 - iii. Have high traffic with higher truck traffic and movement of goods and services than Collector roadways.
- 17. When a roadway segment is below threshold for resurfacing based on roadway classification (F&I #15 & 16), multiple steps are taken to select the preferred treatment:
 - a. **Existing curb condition and curb reveal:** T&PW will build a tailored treatment to re-establish curb reveal. This is projected to build a deficit list of roadways that needs full depth reconstruction (FDR). This type of work will have to be bid separately. When a road is put in the FDR list, it will continue to receive patching treatments to maintain an acceptable PCI (above threshold) until funding is adequate to move into FDR work and contracts.
 - b. **Subsurface conditions:** Many of ACC roadways lack adequate subsurface compaction resulting in a short service life for roadway pavement. Roads with failed or inadequate subsurface condition that do not qualify for traditional resurfacing will be placed on the FDR list of roadways. When a road is put in the FDR list, it will continue to receive patching treatments to maintain an acceptable PCI (above threshold) until funding is adequate to move into FDR work and contracts.
 - c. **Pavement design based on roadway classification and traffic loading:** A design matrix is in development to create a systemic design process for selecting the mill and inlay thickness that is based on the subsurface conditions and traffic loading with the intent to re-establish curb reveal and proper drainage in the roadway.
- 18. The resurfacing portion of the current CY24 PMP Project List adopted a geographically broad approach, driven by the very low PCI scores identified in the automated pavement scoring conducted in the summer of 2023 (History #12 through #15). For the CY25 PMP Project List, the resurfacing focus shifts to the northern part of ACCGov, with the goal of improving cost efficiency by minimizing mobilization costs. This will be achieved by dividing ACCGov roads geographically, addressing the most deteriorated roads over two consecutive paving seasons. The proposed CY25 resurfacing work will focus on roads north of West Broad St / Atlanta Hwy, while those south of this line will be included in the CY26 PMP Project List. This strategy, successfully implemented in the CY22 & CY23 PMP Projects, has proven effective in reducing unit costs by lowering mobilization expenses and enhancing the efficiency of project completion.
- 19. The proposed CY25 PMP Project List includes 285 roadway segments totaling approximately 71.74 lane-miles of roadway at an estimated cost of \$6,812,183. The proposed CY25 PMP Project List included in the solicitation for bids (History #20) includes:
 - **Rehabilitation (Resurfacing)** will be performed on 67 roadway segments totaling approximately 17.28 lane-miles of roadway at an estimated cost of \$4,806,257, as shown in **Attachment #1**.

Rehabilitation activities typically include one or more of the following preparatory treatments followed by a structural asphalt overlay (commonly referred to as resurfacing): crack seal, chip seal, full-depth patch, milling and inlay, gutter milling, crack preventative layer, leveling, and full depth replacement, in addition to shoulder buildup for rural roadways.

- **Preventative Maintenance** on 218 roadway segments totaling approximately 54.46 lane-miles of roadway at an estimated cost of \$2,005,926, as shown in **Attachment #1**. This list includes:
 - Post-resurfacing crack sealing performed on entire roadway segments with pavement from 4 to 5 years old.
 - Deep patching with crack sealing performed on localized deteriorated areas within poor and marginal roadways (PCI ≤ 55) which exceeds the Department's in-house capabilities to address.

Preventative maintenance activities are designed to preserve existing asphalt pavements and arrest deterioration before it becomes more serious and costly to repair. The effect of timely preventative maintenance is the extension of the life of roadway pavements, thereby delaying resurfacing and/or reconstruction for several years. Preventative Maintenance activities include one or more of the following treatments: crack seal, chip seal, and patching/repair of high severity potholes, corrugation, and rutting.

20. Epps Bridge Parkway is an arterial 4-lane boulevard extending from Atlanta Hwy to the Oconee/Athens-Clarke County Line and measuring slightly over 9 lane-miles. This roadway has received preventative maintenance over the past 10-years and it has outlasted its life expectancy since it was built in late 1990's (over 25-years old). Per the latest roadway condition rating performed in July of 2023, Epps Bridge Parkway is reaching marginal to poor conditions and is in need of resurfacing (1.5 inches of mill & inlay). The total cost for mill & inlay is estimated to be in the amount of \$13,000,000. On the other hand, Preservation Treatments such as hot in place recycling, thin overlay, and/or micro-resurfacing can be an economical alternative that provides cost savings when compared with traditional mill and inlay. Based on actual CY25 PM Project bids, and with remaining funds (approximately \$1,000,000 – see Budget section), staff will work with procurement staff to solicit viable preservation treatments on Epps Bridge Pkwy, and shall bring recommendations for M&C consideration in a future agendas (summer 2025).
21. In the interest of time, T&PW staff, working with Procurement staff, are taking initial steps to advertise the proposed CY25 PMP Project. All M&C actions taken will be reflected in the advertised bid and through an addendum if needed.
22. A contingency allowance of approximately 5% of the cost estimate in the amount of \$300,000 is proposed to be added to the bid schedule to cover unanticipated work not originally included in the itemized bid. This amount of funding is to address balancing of material quantities for work performed on the proposed CY25 PMP Project list and as shown on **Attachment #1**.

23. T&PW staff will present the proposed CY25 PMP Project List to the TSPLOST Oversight Committee to determine if the CY25 PMP Project List is consistent with the Initial Project Statement for TSPLOST 2023 Project 21. This agenda report will be updated prior to the February 4, 2025 voting meeting.
24. On January 9, 2025, T&PW and Finance staff began the advertisement and solicitation of sealed bids on the proposed CY25 PMP Project, as shown on **Attachment #1**. Opening of bids is currently scheduled for February 13, 2024 and is subject to change per future addendums.
25. On January 14, 2025, in a scheduled work session for M&C, T&PW staff presented proposed list of roadways and associated maintenance activities for CY25 PMP Project along with remaining roads to be prioritized for future CY26 PMP Project.
26. T&PW staff will continue working with the Procurement staff on the advertisement and solicitation of sealed bid from qualified perspective pavement contractors. In addition, if the agenda is approved by M&C, T&PW staff will begin the process of soliciting estimates on preservation treatments to address all or portions of Epps Bridge Parkway and with available funding in Calendar Year 2025. Given the expected value of the construction contract, the bids will be brought before M&C for award consideration as part of the upcoming February-March 2025 agenda cycle.
27. This recommendation supports the following ACCGov Strategic Plan Goals & Strategies:
 - Safely Move Around Athens, Strategy D: *Create more usable and aesthetically pleasing corridor connections between residential and commercial areas*
 - Built and Natural Infrastructure, Strategy C: *Adequate funding for maintenance of existing and newly constructed infrastructure*

DEPARTMENT: Transportation & Public Works

Prepared by: Rani Katreeb, Assistant Director



December 10, 2024

Stephen Bailey
Director

Date:



Josh Hawkins, Capital Projects Director

01/06/2025

Date:

ATTACHMENTS:

Attachment #1: CY25 List of Roadways

Attachment #2: Project Resolution

Attachment #3: KCI 2023 Pavement Evaluation and Management Plan Report

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY2025 PMP - REHABILITATION RESURFACING
PROPOSED

REHABILITATION: Structural asphalt overlay (resurfacing) including preparatory treatments.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
1	SEGID-2248	Atkinson Dr	Clifton Dr	Pendleton Dr	43	0.10	0.234	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
2	SEGID-2249	Atkinson Dr	Pendleton Dr	Devereux Dr	47	0.10	0.225	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
3	SEGID-2250	Atkinson Dr	Devereux Dr	Pendleton Dr	51	0.10	0.248	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
4	SEGID-2426	Atkinson Dr	Pendleton Dr	Ashton Dr	55	0.04	0.093	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
5	SEGID-1452	Avery St	Dixon Dr	N Main St	44	0.12	0.236	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
6	SEGID-1551	Bach Dr	Pasteur Ct	Robert Hardeman Rd	43	0.11	0.244	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
7	SEGID-1488	Bach Dr	Picasso Dr	Pasteur Ct	54	0.06	0.148	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
8	SEGID-5975	Brittany Rd	Chesterfield Rd	Chesterfield Rd	42	0.24	0.527	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
9	SEGID-1933	Cameron Ct	WEST END	Vaughn Rd	46	0.10	0.246	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
10	SEGID-445	Carrol Ave	Kntingham Dr	Kntingham Dr	49	0.14	0.312	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
11	SEGID-443	Carrol Ave	Kntingham Dr	NE END	50	0.07	0.168	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
12	SEGID-237	Chapel Hill Way	Providence Rd	SE END	47	0.05	0.125	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
13	SEGID-560	Chattooga Ave	Alley	Nacoochee Ave	41	0.09	0.153	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
14	SEGID-1785	Chelsea Ct	Chesterfield Rd	EAST END	44	0.05	0.149	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
15	SEGID-5151	Cheyenne Ave	Cassidy Cir	Lawton Ln	39	0.18	0.331	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
16	SEGID-5152	Cheyenne Ave	SW END	Cassidy Cir	48	0.19	0.412	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
17	SEGID-3630	Crawford Ave	SE END	Cleveland Ave	45	0.08	0.154	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
18	SEGID-278	Creekwood Pl	SOUTH END	Creekwood Dr	45	0.09	0.224	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
19	SEGID-2914	Crestview Cir	Belvoir Hts	Pine Needle Rd	39	0.20	0.369	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
20	SEGID-2779	Deer Creek Ct	SOUTH END	Vincent Dr	43	0.10	0.281	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
21	SEGID-2874	Easy St	Satula Ave	Park Ave	48	0.09	0.170	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
22	SEGID-1796	Eton Ct	Yorkshire Rd	NE END	43	0.05	0.158	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
23	SEGID-2194	Ferncliff Dr	Rivermont Rd	Woodcrest Dr	43	0.09	0.184	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
24	SEGID-2201	Ferncliff Dr	Robinhood Ct	Rivermont Rd	43	0.12	0.230	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
25	SEGID-2200	Ferncliff Dr	Woodcrest Dr	Robinhood Ct	46	0.09	0.186	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
26	SEGID-2206	Ferncliff Dr	Rivermont Rd	Pine Forest Dr	60	0.07	0.147	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
27	SEGID-1632	Hanover Ct	Hanover Dr	SE END	41	0.08	0.160	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
28	SEGID-2972	Hart Ave	King Ave	Hodgson Dr	42	0.22	0.440	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
29	SEGID-146	Hobbs Rd	Woodside Dr	Woodside Dr	43	0.53	0.826	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
30	SEGID-561	Hodgson Dr	W Broad St	Hart Ave	43	0.03	0.089	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
31	SEGID-2971	Hodgson Dr	Hart Ave	King Ave	46	0.24	0.450	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
32	SEGID-95	Joe Carey Dr	SE END	Dunlap Rd	46	0.10	0.160	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
33	SEGID-2863	Lakeview St	Nantahala Ave	New Ave	49	0.05	0.086	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
34	SEGID-456	Landor Pl	Forest Heights Dr	NW END	49	0.05	0.184	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
35	SEGID-723	Maxey Blvd	Barber St	EAST END	41	0.18	0.439	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
36	SEGID-2045	Merlin Dr	Lavender Rd	Merlin Ct	49	0.44	1.009	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
37	SEGID-312	Michele Pl	Michele Dr	Lake Dr	46	0.09	0.179	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
38	SEGID-1604	Montclair Ct	WEST END	Bob Wages Rd	49	0.06	0.173	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
39	SEGID-2717	Nassau Ln	Championship Dr	Fairway Dr	40	0.07	0.135	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
40	SEGID-1540	Nassau Ln	Putter Pl	Club Dr	45	0.13	0.233	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
41	SEGID-1539	Nassau Ln	Fairway Dr	Putter Pl	51	0.13	0.260	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
42	SEGID-2718	Nassau Ln	Club Dr	Championship Dr	55	0.15	0.290	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
43	SEGID-5380	Oakmont Cir	Moores Grove Rd	Oakmont Pl	38	0.22	0.397	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
44	SEGID-5378	Oakmont Cir	Oakmont Pl	Moores Grove Rd	49	0.16	0.307	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
45	SEGID-5379	Oakmont Pl	Oakmont Cir	NORTH END	48	0.05	0.142	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY2025 PMP - REHABILITATION RESURFACING
PROPOSED

REHABILITATION: Structural asphalt overlay (resurfacing) including preparatory treatments.

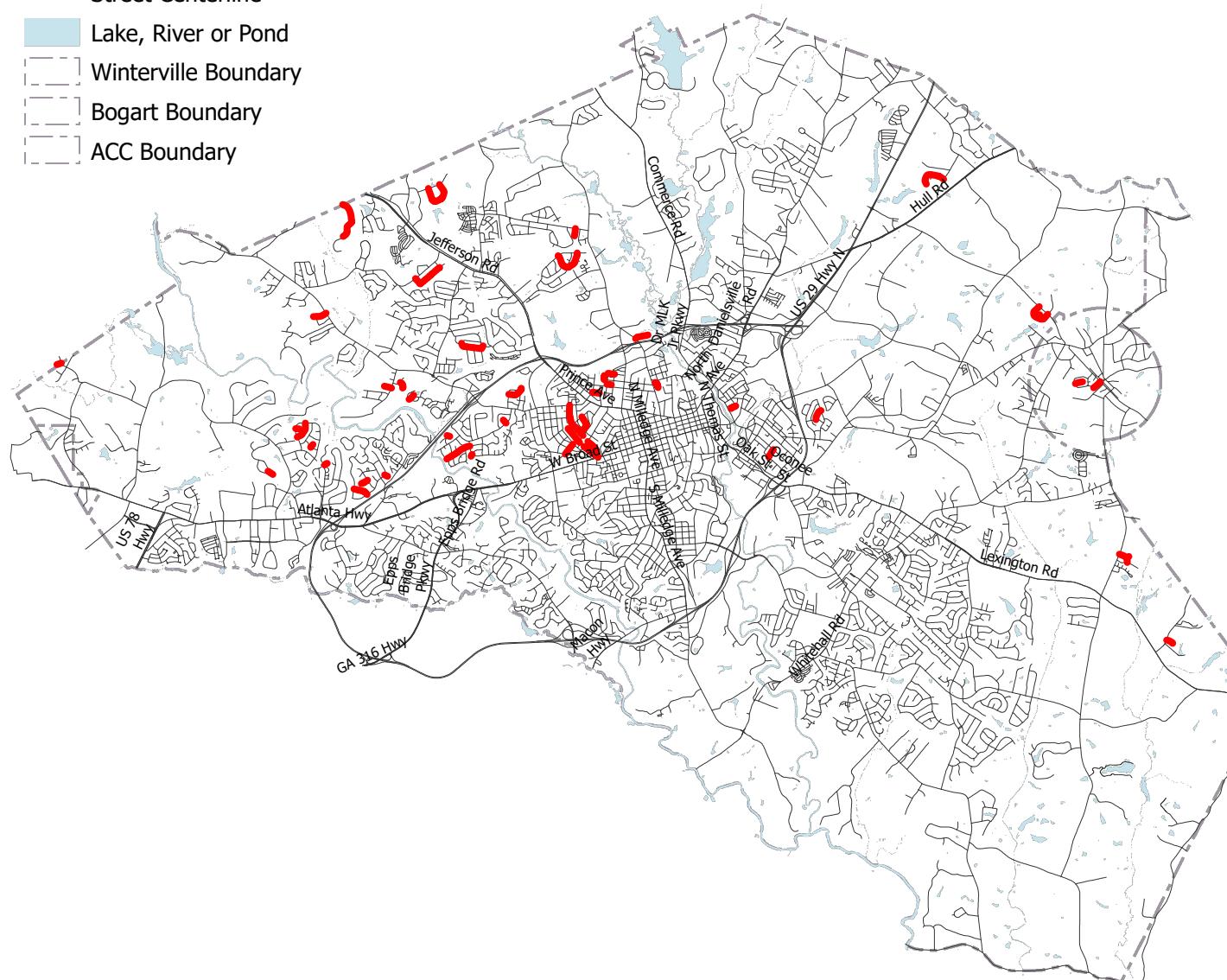
No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
46	SEGID-1477	Pasteur Ct	Bach Dr	NE END	46	0.04	0.144	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
47	SEGID-387	Pine Forest Ct	SW END	Pine Forest Dr	48	0.03	0.168	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
48	SEGID-155	Ramble Hills Dr	Ramble Hills Way	Tallassee Rd	42	0.14	0.270	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
49	SEGID-154	Ramble Hills Dr	Ramble Hills Way	W End	45	0.04	0.080	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
50	SEGID-394	Rockdale Ct	Escoe Ct	EAST END	39	0.04	0.134	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
51	SEGID-4020	Simmons St	Branch St	E Broad St	38	0.12	0.217	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
52	SEGID-164	Slick Rock Ct	Huntington Shoals Dr	NE END	44	0.04	0.143	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
53	SEGID-220	St Paul Dr	Norman Way	Providence Rd	41	0.13	0.254	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
54	SEGID-219	St Paul Dr	Riverbottom Rd	Norman Way	42	0.07	0.129	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
55	SEGID-3924	Summerwood Pl	SW END	Pearl St	48	0.07	0.203	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
56	SEGID-2959	Sunset Ter	Old W Broad St	Sunset Dr	42	0.09	0.187	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
57	SEGID-2958	Sunset Ter	Sunset Dr	Holman Ave	53	0.08	0.145	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
58	SEGID-1453	Susan Cir	Winterhills Dr	E End	39	0.10	0.222	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
59	SEGID-2035	Thornhill Cir	Thornhill Dr	Thornhill Dr	43	0.52	1.094	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
60	SEGID-2864	Tibbetts Ave	Hiawassee Ave	EAST END	49	0.06	0.104	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
61	SEGID-225	Urbana Way	SW END	New Haven Dr	43	0.10	0.227	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
62	SEGID-2955	Westover Dr	Talmadge Dr	Pine Needle Rd	48	0.24	0.467	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
63	SEGID-2965	Willow Run	Talmadge Dr	Pine Needle Rd	40	0.21	0.375	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
64	SEGID-2923	Willow Run	Pine Needle Rd	Edwards Dr	46	0.11	0.197	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
65	SEGID-2940	Willow Run	Oglethorpe Ter	Oglethorpe Ave	50	0.08	0.146	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
66	SEGID-2933	Willow Run	Edwards Dr	Oglethorpe Ter	56	0.08	0.154	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
67	SEGID-4342	Wright Cir	Cook Dr	Washington Dr	48	0.15	0.301	Roadway - 20% Patching + 1.5" 9.5 MM Asphalt Mill & Inlay - Local
TOTAL						8.26	17.28	

CY 2025 PMP - RESURFACING NORTH



Legend

- CY25 PMP**
Resurfacing North
- Street Centerline
- Lake, River or Pond
- Winterville Boundary
- Bogart Boundary
- ACC Boundary



0 1.25 2.5 5 Miles



Athens-Clarke County
Transportation & Public Works

Current Time: 11/25/2024

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / PATCH ONLY
PROPOSED

PREVENTATIVE MAINTENANCE: Includes Patching on localized deteriorations including large potholes, corrugations, and rutting of pavement.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
1	SEGID-5509	Atlanta Hwy	Old Cleveland Rd	Cleveland Rd	56	0.04	0.121	Patch Only
2	SEGID-5515	Atlanta Hwy	Dozer Dr	Nunnally St	58	0.32	1.159	Patch Only
3	SEGID-6076	Atlanta Hwy	Cleveland Rd	Dozer Dr	58	0.06	0.220	Patch Only
4	SEGID-4000	Baldwin St	E Campus Rd	Williams St	51	0.09	0.281	Patch Only
5	SEGID-3421	Baxter St	S Rocksprings St	Clarke Central HS	57	0.17	0.504	Patch Only
6	SEGID-3013	Baxter St	Magnolia St	Dudley Dr	57	0.04	0.126	Patch Only
7	SEGID-4914	Cedar Shoals Dr	Whit Davis Rd	Redstone Dr	50	0.18	0.590	Patch Only
8	SEGID-4256	College Station Rd	Mark Twain Cir	Mark Twain Cir	50	0.11	0.441	Patch Only
9	SEGID-4101	College Station Rd	E Campus Rd	Carlton St	51	0.18	0.677	Patch Only
10	SEGID-4249	College Station Rd	University Cir	Segrest Cir	51	0.23	0.914	Patch Only
11	SEGID-4257	College Station Rd	College Cir	Mark Twain Cir	51	0.06	0.246	Patch Only
12	SEGID-3828	Danielsville Rd	Northside Dr	Alley	58	0.01	0.024	Patch Only
13	SEGID-4411	Danielsville Rd	Forest Acres Cir	Helican Springs Rd	58	0.02	0.066	Patch Only
14	SEGID-2174	Epps Bridge Pkwy	Kirkwood Dr	Timothy Rd	56	0.10	0.403	Patch Only
15	SEGID-2176	Epps Bridge Pkwy	Telfair Pl	Kirkwood Dr	56	0.09	0.266	Patch Only
16	SEGID-2177	Epps Bridge Pkwy	Windy Hill Pl	Old Epps Bridge Rd	56	0.21	0.783	Patch Only
17	SEGID-2131	Epps Bridge Pkwy	Ridgeview Dr	Iris Dr	58	0.11	0.404	Patch Only
18	SEGID-2179	Epps Bridge Pkwy	Wood Lake Dr	Telfair Pl	58	0.21	0.736	Patch Only
19	SEGID-5693	Epps Bridge Pkwy	Timothy Rd	Ruthwood Ln	58	0.14	0.425	Patch Only
20	SEGID-4536	Gaines School Rd	Maison Dr	Continental Dr	56	0.06	0.228	Patch Only
21	SEGID-5087	Gaines School Rd	Continental Dr	Lexington Rd	56	0.17	0.903	Patch Only
22	SEGID-5792	Gaines School Rd	Cedar Creek Dr	Ashley Cir	56	0.06	0.229	Patch Only
23	SEGID-4535	Gaines School Rd	Briarcliff Ln	Maison Dr	57	0.03	0.117	Patch Only
24	SEGID-4545	Gaines School Rd	Cedar Pointe Dr	Springtree Rd	58	0.02	0.075	Patch Only
25	SEGID-5794	Gaines School Rd	Private Rd	Private Rd	58	0.03	0.092	Patch Only
26	SEGID-4639	Greencrest Dr	Barnett Shoals Rd	Brookwood Dr	49	0.15	0.317	Patch Only
27	SEGID-5806	Hawthorne Ave	Hawthorne Park	Prince Ave	58	0.22	0.897	Patch Only
28	SEGID-4426	Ila Rd	Norwood Cir	Norwood Ln	57	0.10	0.180	Patch Only
29	SEGID-2976	King Ave	Old W Broad St	Sunset Dr	51	0.10	0.187	Patch Only
30	SEGID-679	N Chase St	Reese St	W Hancock Ave	42	0.08	0.152	Patch Only
31	SEGID-1282	N Thomas St	E Strong St	North Ave	57	0.05	0.204	Patch Only
32	SEGID-3867	North Ave	Northcrest Dr	Entrance Ramp	58	0.02	0.114	Patch Only
33	SEGID-4453	Olympic Dr	Indian Hills Dr	Indian Hills Dr	49	0.58	2.857	Patch Only

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / PATCH ONLY
PROPOSED

PREVENTATIVE MAINTENANCE: Includes Patching on localized deteriorations including large potholes, corrugations, and rutting of pavement.

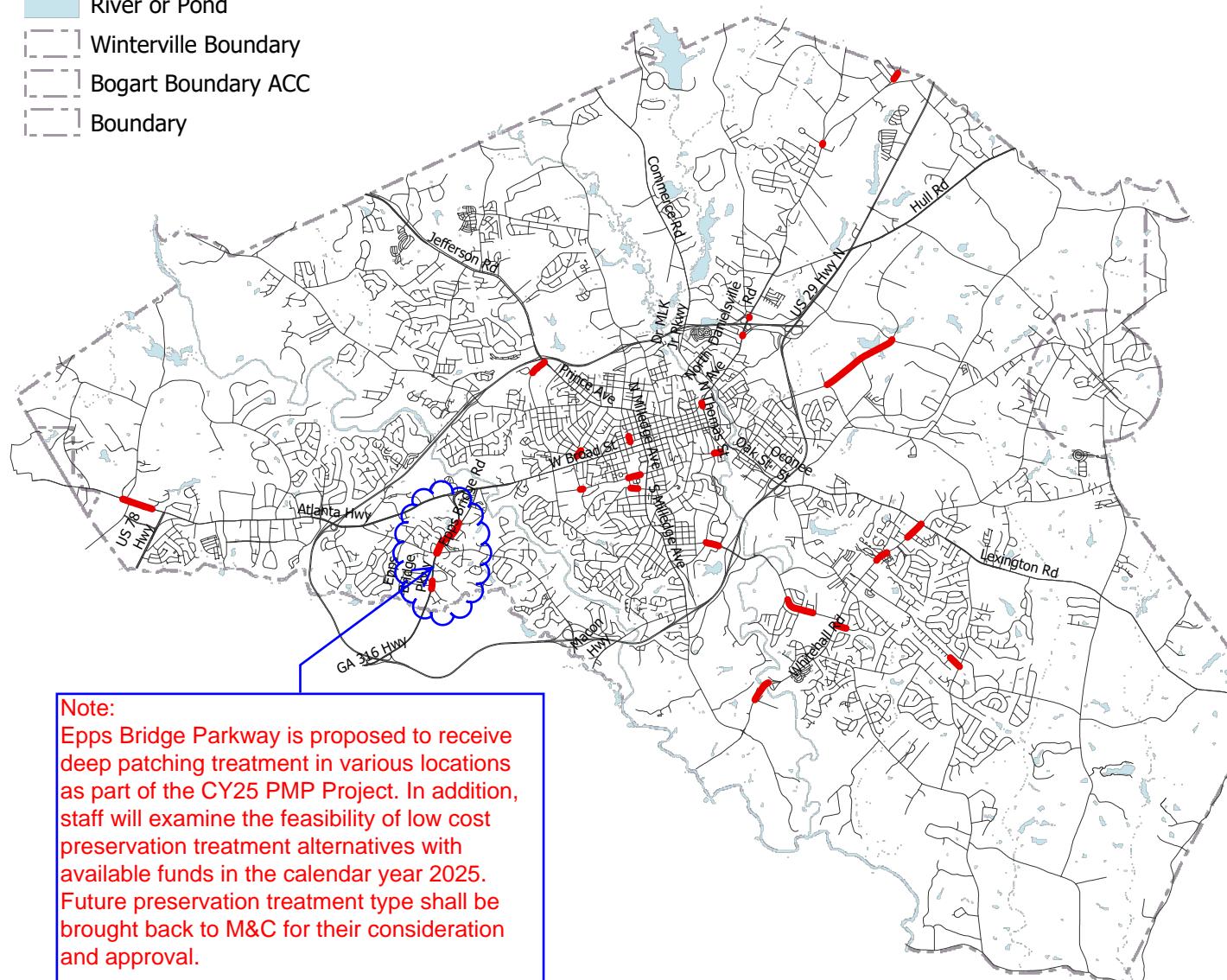
No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
34	SEGID-4451	Olympic Dr	Hancock Industrial Way	Hancock Industrial Way	50	0.50	2.274	Patch Only
35	SEGID-3414	Springdale St	S Rocksprings St	Myrna Ct	50	0.10	0.244	Patch Only
36	SEGID-4138	Whitehall Rd	Hidden Hills Ln	White Cir	57	0.30	0.572	Patch Only
								4.93 18.03

CY 2025 PMP - PATCH ONLY



Legend

- CY25 PMP Patch Only
- Street Centerline Lake,
- River or Pond
- Winterville Boundary
- Bogart Boundary ACC
- Boundary



0 1.25 2.5 5 Miles



Athens-Clarke County
Transportation & Public Works

Current Time: 11/25/2024

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / POST RESURFACING CRACK SEALING
PROPOSED

PREVENTATIVE MAINTENANCE: Post 4 year old pavement crack seal.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
1	SEGID-5373	Adair St	Athens Rd	Harris St	92	0.18	0.350	Post Resurfacing Crack Seal
2	SEGID-3591	Barrow St	Barber St	Childs St	92	0.09	0.183	Post Resurfacing Crack Seal
3	SEGID-3595	Barrow St	Childs St	Pulaski St	93	0.11	0.220	Post Resurfacing Crack Seal
4	SEGID-4478	Ben Epps Dr	Ben Epps Dr	Lyons Rd	83	0.10	0.205	Post Resurfacing Crack Seal
5	SEGID-4479	Ben Epps Dr	Lyons Rd	Winterville Rd	95	0.03	0.059	Post Resurfacing Crack Seal
6	SEGID-599	Benning St	Columbus Ave	Paris St	91	0.04	0.086	Post Resurfacing Crack Seal
7	SEGID-408	Bethany Ct	Kirby Ln	EAST END	94	0.09	0.240	Post Resurfacing Crack Seal
8	SEGID-409	Bethany Ct	Carrie Ct	Kirby Ln	94	0.12	0.218	Post Resurfacing Crack Seal
9	SEGID-2237	Bowden Ln	Carrie Ct	Sarah Dr	59	0.04	0.072	Post Resurfacing Crack Seal
10	SEGID-2235	Bowden Ln	Kara Dr	Kelli Dr	90	0.07	0.121	Post Resurfacing Crack Seal
11	SEGID-2236	Bowden Ln	Kelli Dr	Carrie Ct	95	0.02	0.042	Post Resurfacing Crack Seal
12	SEGID-4376	Buford Way	SOUTH END	Burkland Dr	90	0.07	0.193	Post Resurfacing Crack Seal
13	SEGID-3396	Burnett St	Pinecrest Dr	OFarrell St	77	0.09	0.167	Post Resurfacing Crack Seal
14	SEGID-3399	Burnett St	OFarrell St	Scott St	94	0.07	0.132	Post Resurfacing Crack Seal
15	SEGID-3614	Cain St	WEST END	Lyndon Ave	62	0.09	0.150	Post Resurfacing Crack Seal
16	SEGID-2406	Camak Dr	Unknown	Old Jefferson Rd	78	0.13	0.195	Post Resurfacing Crack Seal
17	SEGID-5730	Camak Dr	Andover Dr	Timsbury Dr	84	0.04	0.059	Post Resurfacing Crack Seal
18	SEGID-2405	Camak Dr	Canady Dr	Andover Dr	92	0.08	0.129	Post Resurfacing Crack Seal
19	SEGID-2404	Camak Dr	Jefferson River Rd	Canady Dr	94	0.10	0.165	Post Resurfacing Crack Seal
20	SEGID-1948	Camelot Dr	SW END	Quail Run Dr	95	0.08	0.161	Post Resurfacing Crack Seal
21	SEGID-2403	Canady Dr	Camak Dr	NORTH END	91	0.14	0.247	Post Resurfacing Crack Seal
22	SEGID-407	Carrie Ct	Bowden Ln	Bethany Ct	95	0.07	0.120	Post Resurfacing Crack Seal
23	SEGID-90	Carrie Ct	Bethany Ct	NORTH END	95	0.03	0.105	Post Resurfacing Crack Seal
24	SEGID-3339	Castalia Ave	Milledge Cir	Oakland Ave	84	0.15	0.278	Post Resurfacing Crack Seal
25	SEGID-4385	Catalpa Ln	Lombardy Dr	Catalpa Dr	91	0.13	0.241	Post Resurfacing Crack Seal
26	SEGID-3764	Catalpa Way	Catalpa Ct	Lombardy Dr	91	0.09	0.174	Post Resurfacing Crack Seal
27	SEGID-3766	Catalpa Way	Burkland Dr	Catalpa Ct	92	0.18	0.341	Post Resurfacing Crack Seal
28	SEGID-292	Chalice Ct	SOUTH END	Chalice Ln	90	0.03	0.087	Post Resurfacing Crack Seal
29	SEGID-291	Chalice Ct	Chalice Ln	NORTH END	93	0.04	0.097	Post Resurfacing Crack Seal
30	SEGID-2710	Championship Dr	Club Dr	Tee Ln	79	0.13	0.271	Post Resurfacing Crack Seal
31	SEGID-2716	Championship Dr	Bunker Dr	Nassau Ln	79	0.13	0.291	Post Resurfacing Crack Seal
32	SEGID-2713	Championship Dr	Tee Ln	Bunker Dr	85	0.10	0.214	Post Resurfacing Crack Seal
33	SEGID-2743	Chesterton Dr	Chesterton Way	Acadia Dr	59	0.09	0.166	Post Resurfacing Crack Seal
34	SEGID-3592	Childs St	Prince Ave	Barrow St	94	0.21	0.429	Post Resurfacing Crack Seal

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / POST RESURFACING CRACK SEALING
PROPOSED

PREVENTATIVE MAINTENANCE: Post 4 year old pavement crack seal.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
35	SEGID-1374	Dairy Pak Rd	Barber St	EAST END	77	0.36	0.841	Post Resurfacing Crack Seal
36	SEGID-4024	Derby St	E Broad St	Arch St	94	0.12	0.256	Post Resurfacing Crack Seal
37	SEGID-2725	Deupree Ct	Pepperidge Ln	NORTH END	89	0.05	0.135	Post Resurfacing Crack Seal
38	SEGID-4281	Edgewater Dr	Dekie Dr	SE End	90	0.21	0.541	Post Resurfacing Crack Seal
39	SEGID-4287	Edgewater Dr	Indiana Ave	Dekie Dr	93	0.19	0.472	Post Resurfacing Crack Seal
40	SEGID-719	Elizabeth St	SOUTH END	Willow St	87	0.05	0.101	Post Resurfacing Crack Seal
41	SEGID-84	Fairway Ter	NW END	Fairway Dr	89	0.04	0.139	Post Resurfacing Crack Seal
42	SEGID-3554	Franklin St	Meigs St	Hill St	83	0.08	0.145	Post Resurfacing Crack Seal
43	SEGID-3520	Franklin St	Hill St	Cobb St	87	0.08	0.119	Post Resurfacing Crack Seal
44	SEGID-3521	Franklin St	Cobb St	Prince Ave	91	0.12	0.159	Post Resurfacing Crack Seal
45	SEGID-3553	Franklin St	W Hancock Ave	Meigs St	92	0.08	0.116	Post Resurfacing Crack Seal
46	SEGID-770	Freeman Cir	Boley Dr	NW END	91	0.21	0.319	Post Resurfacing Crack Seal
47	SEGID-1457	Gatewood Pl	Gatewood Cir	EAST END	91	0.15	0.223	Post Resurfacing Crack Seal
48	SEGID-4718	Greenwood Ln	Lea Ct	Rebecca Pl	91	0.15	0.309	Post Resurfacing Crack Seal
49	SEGID-649	Henderson Ext	Dallas St	S Rocksprings St	81	0.10	0.199	Post Resurfacing Crack Seal
50	SEGID-6030	Henry Meyer Rd	Cherokee Rd	Springbrook Dr	89	0.17	0.332	Post Resurfacing Crack Seal
51	SEGID-573	Hills Chapel St	W Hancock Ave	Brooklyn Ave	83	0.09	0.164	Post Resurfacing Crack Seal
52	SEGID-574	Hills Chapel St	Brooklyn Ave	Minor St	90	0.04	0.069	Post Resurfacing Crack Seal
53	SEGID-2549	Huntleys Gln	Township Ln	Towns Walk Dr	93	0.06	0.088	Post Resurfacing Crack Seal
54	SEGID-4293	Indiana Ave	Kentucky Cir	Lexington Rd	90	0.10	0.212	Post Resurfacing Crack Seal
55	SEGID-4296	Indiana Ave	Edgewater Dr	Kentucky Cir	94	0.19	0.344	Post Resurfacing Crack Seal
56	SEGID-4291	Indiana Ave	Edgewater Dr	Johnson Dr	95	0.08	0.119	Post Resurfacing Crack Seal
57	SEGID-4290	Johnson Dr	Barnett Shoals Rd	Indiana Ave	91	0.15	0.261	Post Resurfacing Crack Seal
58	SEGID-2242	Kelli Dr	Kirby Ln	Bowden Ln	85	0.12	0.198	Post Resurfacing Crack Seal
59	SEGID-230	Kingston Way	Providence Rd	NW END	93	0.10	0.240	Post Resurfacing Crack Seal
60	SEGID-2243	Kirby Ln	Kelli Dr	Sarah Dr	92	0.06	0.103	Post Resurfacing Crack Seal
61	SEGID-2245	Kirby Ln	Sarah Dr	Bowden Ln	93	0.16	0.298	Post Resurfacing Crack Seal
62	SEGID-2247	Kirby Ln	Bowden Ln	Bethany Ct	93	0.07	0.108	Post Resurfacing Crack Seal
63	SEGID-316	Lake Ct	Lake Pl	SE END	91	0.02	0.085	Post Resurfacing Crack Seal
64	SEGID-317	Lake Pl	Lake Dr	Lake Ct	85	0.06	0.121	Post Resurfacing Crack Seal
65	SEGID-315	Lake Pl	Lake Ct	EAST END	93	0.08	0.214	Post Resurfacing Crack Seal
66	SEGID-5700	Lake Vista Dr	Lakeland Dr	Lake Vista Way	92	0.13	0.252	Post Resurfacing Crack Seal
67	SEGID-2007	Lake Vista Way	SOUTH END	Lake Vista Dr	90	0.02	0.102	Post Resurfacing Crack Seal
68	SEGID-5699	Lakeland Dr	Lake Vista Dr	Lakeland Way	91	0.05	0.113	Post Resurfacing Crack Seal

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / POST RESURFACING CRACK SEALING
PROPOSED

PREVENTATIVE MAINTENANCE: Post 4 year old pavement crack seal.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
69	SEGID-2008	Lakeland Dr	Lakeland Way	Lakeland Ct	92	0.14	0.277	Post Resurfacing Crack Seal
70	SEGID-2002	Lakeland Dr	Lakeland Ct	Lake Vista Way	95	0.03	0.054	Post Resurfacing Crack Seal
71	SEGID-2005	Lakeland Dr	Lake Vista Way	Gatewood Cir	95	0.07	0.149	Post Resurfacing Crack Seal
72	SEGID-2006	Lakeland Dr	SOUTH END	Lake Vista Dr	95	0.03	0.110	Post Resurfacing Crack Seal
73	SEGID-2010	Lakeland Dr	Gatewood Cir	Oakwood Ter	95	0.09	0.433	Post Resurfacing Crack Seal
74	SEGID-2012	Lakeland Dr	Oakwood Ter	Lakeland Pl	95	0.09	0.417	Post Resurfacing Crack Seal
75	SEGID-2013	Lakeland Pl	WEST END	Lakeland Dr	93	0.11	0.255	Post Resurfacing Crack Seal
76	SEGID-2001	Lakeland Way	Lakeland Dr	EAST END	93	0.04	0.138	Post Resurfacing Crack Seal
77	SEGID-4716	Lauren Ln	Sorenson Rdg	Rebecca Pl	93	0.12	0.228	Post Resurfacing Crack Seal
78	SEGID-6044	Le Conte Ave	Hampton Ct	University Dr	83	0.09	0.194	Post Resurfacing Crack Seal
79	SEGID-6046	Le Conte Ave	Southview Dr	Hampton Ct	87	0.08	0.162	Post Resurfacing Crack Seal
80	SEGID-4669	Lindmar Ct	SW END	Ashbrook Dr	90	0.04	0.146	Post Resurfacing Crack Seal
81	SEGID-2728	Linnea Way	Pepperidge Ln	Chesterton Dr	91	0.10	0.208	Post Resurfacing Crack Seal
82	SEGID-1740	Live Oak Dr	Lavender Rd	Live Oak Ct	91	0.12	0.224	Post Resurfacing Crack Seal
83	SEGID-1739	Live Oak Dr	Live Oak Ct	Live Oak Pl	93	0.04	0.076	Post Resurfacing Crack Seal
84	SEGID-1738	Live Oak Dr	Live Oak Pl	Live Oak Cir	95	0.09	0.162	Post Resurfacing Crack Seal
85	SEGID-151	Live Oak Pl	Rustwood Dr	Live Oak Dr	73	0.11	0.166	Post Resurfacing Crack Seal
86	SEGID-4327	Luther Ln	Martin Cir	Washington Dr	94	0.09	0.168	Post Resurfacing Crack Seal
87	SEGID-827	Madison Hts	Strickland Ave	North Ave	89	0.17	0.302	Post Resurfacing Crack Seal
88	SEGID-3690	Mandy Dr	Dr MLK Jr Pkwy	Peeks Pt	88	0.10	0.157	Post Resurfacing Crack Seal
89	SEGID-3692	Mandy Dr	Peeks Pt	Trio Ln	95	0.11	0.241	Post Resurfacing Crack Seal
90	SEGID-4255	Mark Twain Cir	College Station Rd	Faulkner St	80	0.04	0.066	Post Resurfacing Crack Seal
91	SEGID-4262	Mark Twain Cir	Sandburg St	NORTH END	83	0.05	0.094	Post Resurfacing Crack Seal
92	SEGID-4253	Mark Twain Cir	Faulkner St	Sandburg St	95	0.06	0.124	Post Resurfacing Crack Seal
93	SEGID-89	Maynard Ct	Hunt Rd	NE END	90	0.08	0.125	Post Resurfacing Crack Seal
94	SEGID-5022	Meadow Creek Dr	WEST END	McCrackin Ct	92	0.19	0.457	Post Resurfacing Crack Seal
95	SEGID-5019	Meadow Creek Dr	Meadow Creek Ln	Whit Davis Rd	94	0.18	0.294	Post Resurfacing Crack Seal
96	SEGID-5024	Meadow Creek Dr	McCrackin Ct	Meadow Creek Ln	94	0.08	0.139	Post Resurfacing Crack Seal
97	SEGID-5020	Meadow Creek Ln	Meadow Creek Dr	EAST END	92	0.16	0.413	Post Resurfacing Crack Seal
98	SEGID-339	Melba Ln	Russell Way	Russell Way	94	0.13	0.379	Post Resurfacing Crack Seal
99	SEGID-1773	Melbourne Pl	Melbourne Dr	SE END	91	0.09	0.228	Post Resurfacing Crack Seal
100	SEGID-3211	Milledge Avenue Ext	Rogers Rd	E Campus Rd	85	0.25	0.364	Post Resurfacing Crack Seal
101	SEGID-3212	Milledge Avenue Ext	Rogers Rd	Rogers Rd	89	0.23	0.389	Post Resurfacing Crack Seal
102	SEGID-1958	Mint Pl	Wisteria Way	NE END	89	0.06	0.166	Post Resurfacing Crack Seal

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / POST RESURFACING CRACK SEALING
PROPOSED

PREVENTATIVE MAINTENANCE: Post 4 year old pavement crack seal.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
103	SEGID-2846	Nantahala Ext	Pound St	Buena Vista Ave	92	0.11	0.170	Post Resurfacing Crack Seal
104	SEGID-3389	Northview Dr	Hillside Ct	Pinecrest Dr	85	0.06	0.115	Post Resurfacing Crack Seal
105	SEGID-3391	Northview Dr	Woodrow St	Hillside Ct	94	0.12	0.204	Post Resurfacing Crack Seal
106	SEGID-3333	Northview Dr	Morton Ave	Woodrow St	95	0.05	0.102	Post Resurfacing Crack Seal
107	SEGID-3105	Oakdale Rd	Duncan Springs Rd	Terrell Dr	94	0.12	0.222	Post Resurfacing Crack Seal
108	SEGID-728	Old Commerce Dr	Old Commerce Rd	Commerce Rd	92	0.20	0.349	Post Resurfacing Crack Seal
109	SEGID-3715	Old Commerce Rd	North Bluff Rd	Commerce Rd	90	0.04	0.067	Post Resurfacing Crack Seal
110	SEGID-4163	Onyx Pl	WEST END	Sapphire Ct	92	0.02	0.109	Post Resurfacing Crack Seal
111	SEGID-4167	Onyx Pl	Sapphire Ct	Diamond Dr	93	0.08	0.183	Post Resurfacing Crack Seal
112	SEGID-2151	Orion Pl	Skyline Pkwy	EAST END	91	0.02	0.093	Post Resurfacing Crack Seal
113	SEGID-3230	Parkway Dr	Parkway Dr	Parkway Dr	75	0.07	0.122	Post Resurfacing Crack Seal
114	SEGID-3228	Parkway Dr	Milledge Hts	Milledge Hts	88	0.06	0.119	Post Resurfacing Crack Seal
115	SEGID-3229	Parkway Dr	Milledge Hts	Milledge Hts	91	0.07	0.149	Post Resurfacing Crack Seal
116	SEGID-3232	Parkway Dr	Milledge Hts	Greenwood Dr	93	0.05	0.104	Post Resurfacing Crack Seal
117	SEGID-5042	Pembrook Ct	SE END	Shadow Moss Dr	82	0.04	0.100	Post Resurfacing Crack Seal
118	SEGID-542	Pound St	Boulevard	Nantahala Ext	90	0.07	0.166	Post Resurfacing Crack Seal
119	SEGID-1021	Produce Ln	Winterville Rd	NW END	89	0.09	0.286	Post Resurfacing Crack Seal
120	SEGID-1947	Quail Run Dr	Camelot Dr	NW END	89	0.04	0.067	Post Resurfacing Crack Seal
121	SEGID-1951	Quail Run Dr	Camelot Dr	Mallard Dr	89	0.11	0.205	Post Resurfacing Crack Seal
122	SEGID-2064	Quail Run Dr	Covey Ln	Partridge Ln	90	0.08	0.146	Post Resurfacing Crack Seal
123	SEGID-2061	Quail Run Dr	Mallard Dr	Partridge Ln	92	0.11	0.213	Post Resurfacing Crack Seal
124	SEGID-569	Rose St	Colima Ave	Evans St	81	0.05	0.092	Post Resurfacing Crack Seal
125	SEGID-568	Rose St	Magnolia St	Colima Ave	86	0.08	0.168	Post Resurfacing Crack Seal
126	SEGID-287	Round Table Ct	SE END	Round Table Rd	87	0.06	0.112	Post Resurfacing Crack Seal
127	SEGID-288	Round Table Rd	Round Table Ct	Chalice Ln	78	0.10	0.161	Post Resurfacing Crack Seal
128	SEGID-294	Round Table Rd	Chalice Ln	Roberts Rd	83	0.13	0.264	Post Resurfacing Crack Seal
129	SEGID-286	Round Table Rd	Roberts Rd	Round Table Ct	85	0.09	0.181	Post Resurfacing Crack Seal
130	SEGID-342	Russell Way	Alley	Melba Ln	82	0.05	0.170	Post Resurfacing Crack Seal
131	SEGID-3428	S Harris St	Peabody St	Baxter St	91	0.08	0.143	Post Resurfacing Crack Seal
132	SEGID-4008	S Poplar St	Oak St	Branch St	71	0.17	0.375	Post Resurfacing Crack Seal
133	SEGID-3400	Scott St	Morton Ave	Burnett St	92	0.07	0.138	Post Resurfacing Crack Seal
134	SEGID-6216	Seasons Pass	Winterville Park Ln	Spring Leaf Trl	95	0.26	0.469	Post Resurfacing Crack Seal
135	SEGID-2424	Sharon Cir	Hunnicutt Dr	Sharon Ct	93	0.15	0.347	Post Resurfacing Crack Seal
136	SEGID-2423	Sharon Cir	Sharon Ct	Hunnicutt Dr	100	0.20	0.549	Post Resurfacing Crack Seal

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
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PROPOSED

PREVENTATIVE MAINTENANCE: Post 4 year old pavement crack seal.

No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
137	SEGID-2422	Sharon Ct	SW END	Sharon Cir	95	0.02	0.021	Post Resurfacing Crack Seal
138	SEGID-1962	Silverbell Trce	Wisteria Way	Jasmine Trl	92	0.32	0.695	Post Resurfacing Crack Seal
139	SEGID-2146	Skyline Rdg	SW END	Skyline Pkwy	91	0.08	0.221	Post Resurfacing Crack Seal
140	SEGID-1544	Skyline Way	Skyline Pkwy	SE END	84	0.09	0.222	Post Resurfacing Crack Seal
141	SEGID-282	Southfort Ct	SW END	Westwood Dr	91	0.06	0.136	Post Resurfacing Crack Seal
142	SEGID-4108	Spalding Ct	SOUTH END	Spalding Cir	89	0.10	0.214	Post Resurfacing Crack Seal
143	SEGID-4614	Spring Lake Ct	SE END	Spring Lake Dr	95	0.09	0.185	Post Resurfacing Crack Seal
144	SEGID-4611	Spring Lake Dr	Spring Lake Pt	NW END	92	0.12	0.329	Post Resurfacing Crack Seal
145	SEGID-4615	Spring Lake Dr	Spring Lake Ct	Spring Lake Pt	93	0.08	0.159	Post Resurfacing Crack Seal
146	SEGID-4616	Spring Lake Dr	Cedar Shoals Dr	W Paces Dr	100	0.11	0.248	Post Resurfacing Crack Seal
147	SEGID-4612	Spring Lake Pt	SW END	Spring Lake Dr	95	0.03	0.138	Post Resurfacing Crack Seal
148	SEGID-1578	Spring Leaf Trl	Smokey Rd	Seasons Pass	92	0.12	0.203	Post Resurfacing Crack Seal
149	SEGID-428	Stedman Ct	Ambler Rd	NORTH END	89	0.06	0.124	Post Resurfacing Crack Seal
150	SEGID-3103	Terrell Dr	Duncan Springs Rd	Terrell Ct	84	0.09	0.183	Post Resurfacing Crack Seal
151	SEGID-3104	Terrell Dr	Terrell Ct	Oakdale Rd	89	0.08	0.161	Post Resurfacing Crack Seal
152	SEGID-3108	Terrell Dr	Oakdale Rd	Maple Cir	91	0.10	0.172	Post Resurfacing Crack Seal
153	SEGID-3109	Terrell Dr	Maple Cir	Lullwater Rd	93	0.05	0.108	Post Resurfacing Crack Seal
154	SEGID-2538	Tipperary Ct	NW END	Mal Bay Rd	85	0.06	0.109	Post Resurfacing Crack Seal
155	SEGID-2539	Tipperary Rd	Mal Bay Rd	EAST END	90	0.12	0.216	Post Resurfacing Crack Seal
156	SEGID-2560	Towns Walk Dr	Parrish Pointe Dr	Cliffhaven Ln	53	0.04	0.061	Post Resurfacing Crack Seal
157	SEGID-2564	Towns Walk Dr	Huntleys Gln	Parrish Pointe Dr	71	0.04	0.090	Post Resurfacing Crack Seal
158	SEGID-2544	Towns Walk Dr	Township Ln	Ansbeth Way	93	0.10	0.213	Post Resurfacing Crack Seal
159	SEGID-2548	Towns Walk Dr	Ansbeth Way	Huntleys Gln	95	0.07	0.139	Post Resurfacing Crack Seal
160	SEGID-2546	Township Ln	Ansbeth Way	Huntleys Gln	85	0.03	0.054	Post Resurfacing Crack Seal
161	SEGID-2545	Township Ln	Towns Walk Dr	Ansbeth Way	90	0.14	0.312	Post Resurfacing Crack Seal
162	SEGID-2562	Township Ln	Huntleys Gln	Parrish Pointe Dr	95	0.04	0.094	Post Resurfacing Crack Seal
163	SEGID-1957	Tupelo Ct	Jasmine Trl	NW END	87	0.05	0.158	Post Resurfacing Crack Seal
164	SEGID-4998	Upland Ct	W Paces Dr	NE END	92	0.06	0.151	Post Resurfacing Crack Seal
165	SEGID-4365	Walker Dr	SE END	Danielsville Rd	86	0.21	0.287	Post Resurfacing Crack Seal
166	SEGID-2232	Westchester Dr	Hanover Pl	Tallassee Rd	91	0.21	0.481	Post Resurfacing Crack Seal
167	SEGID-2095	Westchester Dr	Chalfont Dr	Hanover Pl	100	0.04	0.103	Post Resurfacing Crack Seal
168	SEGID-2225	Westchester Dr	Mitchell Bridge Rd	Westchester Ln	100	0.12	0.327	Post Resurfacing Crack Seal
169	SEGID-2226	Westchester Dr	Hanover Pl	Westchester Cir	100	0.02	0.066	Post Resurfacing Crack Seal
170	SEGID-2227	Westchester Dr	Westchester Ln	Hanover Pl	100	0.05	0.123	Post Resurfacing Crack Seal

ATHENS-CLARKE COUNTY TRANSPORTATION AND PUBLIC WORKS
CY25 PMP - PREVENTATIVE MAINTENANCE / POST RESURFACING CRACK SEALING
PROPOSED

PREVENTATIVE MAINTENANCE: Post 4 year old pavement crack seal.

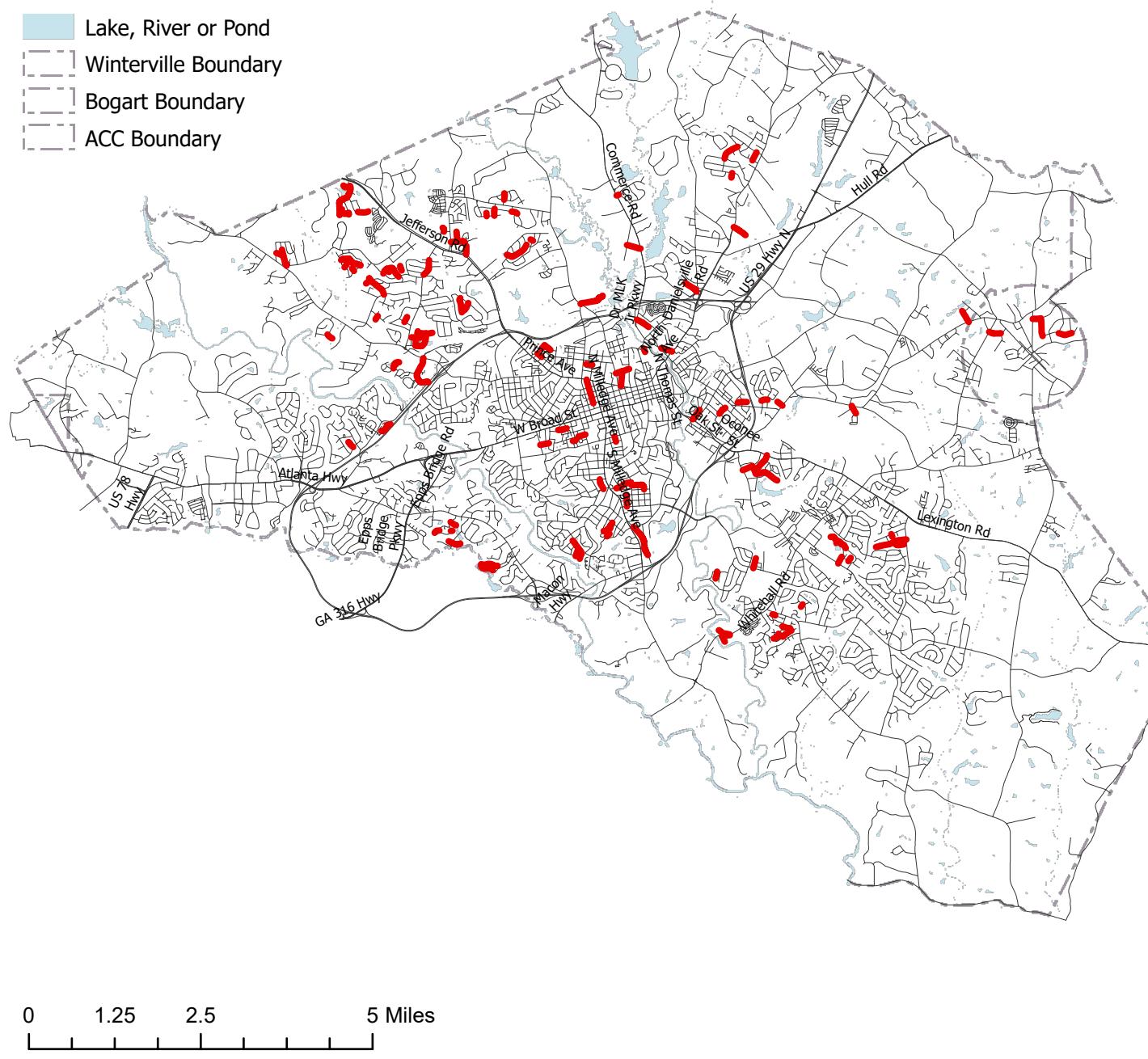
No.	Segment ID	Road Name	From Road	To Road	2023 PCI	Centerline Miles	Lanemiles	Treatment
171	SEGID-2229	Westchester Dr	Westchester Cir	Chalfont Dr	100	0.07	0.136	Post Resurfacing Crack Seal
172	SEGID-940	White Ct	White Cir	White Cir	75	0.20	0.267	Post Resurfacing Crack Seal
173	SEGID-941	White Ter	WEST END	Whitehall Rd	85	0.18	0.240	Post Resurfacing Crack Seal
174	SEGID-414	Whitehead Cir	Whitehead Rd	Rosewood Pl	88	0.14	0.190	Post Resurfacing Crack Seal
175	SEGID-417	Whitehead Cir	Rosewood Pl	Whitehead Ter E	94	0.10	0.158	Post Resurfacing Crack Seal
176	SEGID-3223	Wildwood Ct	Milledge Hts	NE END	86	0.05	0.121	Post Resurfacing Crack Seal
177	SEGID-2749	Winery Way	Harvest Way	Vineyard Dr	49	0.10	0.206	Post Resurfacing Crack Seal
178	SEGID-1576	Winterville Park Ln	Seasons Creek Xing	Seasons Chase	95	0.19	0.318	Post Resurfacing Crack Seal
179	SEGID-4222	Winthrop Ct	Winthrop Way	EAST END	95	0.05	0.154	Post Resurfacing Crack Seal
180	SEGID-2834	Yonah Ave	Pound St	Boulevard Hts	85	0.08	0.143	Post Resurfacing Crack Seal
181	SEGID-4320	Zebulon Dr	WEST END	Cone Dr	69	0.06	0.161	Post Resurfacing Crack Seal
182	SEGID-4319	Zebulon Dr	Cone Dr	EAST END	87	0.07	0.175	Post Resurfacing Crack Seal
						17.79	36.43	

CY 2025 PMP - CRACK SEALING



Legend

- CY25 PMP Post Resurfacing Crack Seal** (Red line)
- Street Centerline** (Black line)
- Lake, River or Pond** (Light blue)
- Winterville Boundary** (Dashed line)
- Bogart Boundary** (Dashed line)
- ACC Boundary** (Dashed line)



Athens-Clarke County
Transportation & Public Works

Current Time: 11/25/2024

**AUTHORIZING RESOLUTION
CY2025 PAVEMENT MAINTENANCE PROGRAM PROJECT**

WHEREAS, The Mayor and Commission of the Unified Government of Athens-Clarke County, Georgia, have specifically determined and found that it is in the public interest to perform rehabilitation/resurfacing on approximately **17.28 (seventeen and twenty-eight one-hundredths)** lane-miles and preventive maintenance on approximately **71.74 (seventy-one and seventy-four one-hundredths)** lane miles of locally maintained roads (hereinafter referred to as “the PMP Project”); and

WHEREAS, The Mayor and Commission of the Unified Government of Athens-Clarke County have further found that the PMP Project is necessary and appropriate for the protection of public safety and serves the general welfare of Athens-Clarke County.

NOW, THEREFORE, the Commission of Athens-Clarke County, Georgia, hereby resolves as follows:

SECTION 1. This PMP Project is authorized and approved as an official project of the Unified Government of Athens-Clarke County, Georgia.

SECTION 2. The Mayor, Clerk of Commission, and Manager, or their respective lawful designees, are hereby expressly authorized to initiate, certify, and sign all documents and to perform all lawful acts necessary to secure funds from the State of Georgia, or the United States of America, or from any department, agency, or subdivision thereof or from any private individual or entity, to defray, in whole or in part, the cost of the said PMP Project; and

SECTION 3. The Manager or his lawful designees are hereby expressly authorized to acquire appropriate interest or title to all real estate necessary for the reasonable execution of said PMP Project, by any means allowed by law including, without limitation, the exercise on behalf of the Unified Government of Athens-Clarke County, of the power of eminent domain;

SECTION 4. The Manager or his lawful designees are hereby authorized to employ or contract for the professional services of attorneys, accountants, engineers, architects, appraisers, sureties, indemnitors, actuaries, and other professional consultants necessary for the plan, design, construction, execution, and general management of the said PMP Project; and,

SECTION 5. The Manager or his lawful designees are hereby authorized to seek bids or competitive proposals in the newspaper, on radio stations and television stations, or by other means as they deem appropriate or as may be required by law; and

SECTION 6. The Mayor, Clerk of Commission, and Manager, or their respective lawful designees, are hereby authorized to execute and file all documents and certifications necessary for the proper completion of the said PMP Project.

SECTION 7. This Resolution shall be construed to confer upon the Mayor, Clerk of Commission, and Manager, or their respective lawful designees, all incidental authority necessary to carry out the purposes of this Resolution as expressed in the preamble and the foregoing sections.

SO RESOLVED on this _____ day of _____, 20____.

APPROVED: _____
Mayor

ATTEST: _____
Clerk of Commission

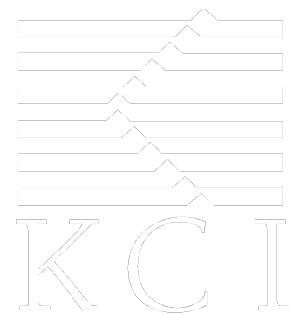


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List of Acronyms

American Society for Testing Materials (ASTM).....	5
Digital Survey Vehicle (DSV).....	5
global positioning system (GPS).....	6
International Roughness Index (IRI).....	14
KCI Technologies Inc. (KCI).....	5
Laser Crack Measurement System (LCMS).....	5
maintenance and rehabilitation (M&R).....	5
Pavement Condition Index (PCI).....	5
Pavement Management System (PMS).....	5

1 Introduction

1.1 Background

Athens-Clarke County contracted KCI Technologies Inc. (KCI) to evaluate pavement conditions and update pavement management plans for County-owned roadways. This involved taking an inventory and assessing the roadway conditions of the entire network within the County limits to determine the best pavement maintenance plan that would bring the highest return on investment.

KCI evaluated about 632 centerline miles of roadway using an automated Laser Crack Measurement System (LCMS) based on the American Society for Testing Materials (ASTM) D6433 Standard Practice for Roads and Parking Lots Pavement Condition Index (PCI) Survey methodology to determine PCI values for each section. KCI also implemented a Pavement Management System (PMS) with the Pavement Express™ platform, predicted future conditions, created budget scenarios, and proposed maintenance and rehabilitation (M&R) plans for the County's roadway pavement network.

1.2 County's Existing Roadway Network

At the beginning of the project, KCI established an inventory and geodatabase of County-owned and maintained roadways with the cooperation of the County's staff. The County provided the street centerline geodatabase for KCI to update it with the required attributes for pavement data collection. About 632 centerline miles (1298 lane-miles) of roadway were identified under the County's jurisdiction.

Table 1. Summary of the Athens-Clarke County's pavement network in May 2023.

Functional Class	Lane-Miles	Number of Sections	% Pavement Network
Principal Arterial	8.8	30	0.7%
Minor Arterial	172.4	528	13.3%
Major Collector	176.5	493	13.7%
Local Road	932.1	3790	72.1%
Alley	8.3	80	0.6%

2 Pavement Condition Data Collection and Processing

2.1 Pavement Condition Data Collection

KCI collected geo-referenced right-of-way images of the County's roadway network using the Digital Survey Vehicle (DSV) between June 8, 2023, and July 20, 2023. KCI's DSV, equipped with the LCMS, shown in Figure 1, has the following features and captures images at 20-ft intervals:

- The LCMS-2 camera is a downward-facing laser array providing images used to evaluate data that conforms with ASTM D6433 protocols, which uses two 1-millimeter-pixel resolution line scan cameras to provide a customized digital condition rating system to collect user-defined severity/extent-based pavement distresses and rutting.
- The pavement distress type, density, severity, and extent are collected with the LCMS-2 and are used to calculate a PCI score between 0-100, representing the condition of 100% of the driven lanes.

- A Point Gray Ladybug 5+ 32MP 360-degree High-Definition camera is utilized for accurate ROW asset capture, extraction, and pavement QC/QA.
- A class 1 inertial profiler simultaneously captures dual-wheel path (left and right) IRI measurements to the hundredth inch, in accordance with AASHTO R48. The inertial profiler meets the requirements and will operate in accordance with AASHTO Standards M 328, R 57-10, R 56-10, and R43M/R43-7.
- Linear distance measuring to within +/-0.5%, and Applanix POS/LV with DGPS (Provides accurate GPS coordinates for each subsystem)

Each image is linearly referenced with the DSV's onboard distance measuring instrument (DMI) and associated global positioning system (GPS) coordinates. For two-lane County highways, KCI collected images in a single direction. KCI collected pavement and right-of-way images from both directions for divided and roads with three or more lanes.



Figure 1. Laser Crack Measurement System for pavement condition assessment.

The LCMS captures enhanced right-of-way images using a right-of-way camera system. The images are used to assess the surface condition of the pavement in accordance with ASTM D6433 Standard Practice for Roads and Parking Lots Pavement Condition Index Surveys.

2.2 Pavement Condition Index (PCI)

PCI is a pavement condition measurement index ranging from 0 to 100. This is an industry-standard defined in ASTM D6433. A newly constructed pavement will have a PCI of 100, whereas a failed pavement will have a PCI of 10 or less, as shown in Figure 2.

PCI Value	Pavement Rating
86 - 100	Excellent
71 - 85	Very Good
56 - 70	Good
41 - 55	Marginal
26 - 40	Poor
0 - 25	Very Poor

Figure 2. Pavement condition category based on the PCI values.

After the initial construction, pavement condition starts deteriorating with time due to traffic loads and volumes, climate, construction materials, and age. Common traffic load-related distresses include fatigue cracking, corner break, etc., whereas block cracking, longitudinal and transverse cracking, etc., are climate-related distress. Typical concrete pavement distresses include transverse cracks, longitudinal cracks, random cracks, corner breaks, spalling, and faulting. A PCI survey allows users to compare all pavements on a standard scale and provides an index for monitoring pavement deterioration and treatment selection during the PMS analysis.

2.3 Field Pilot

KCI worked with the County staff to determine a pilot area of approximately 15 miles of roadways for deliverable review by County staff as shown in Figure 3. The pilot allowed the KCI team to collect, process, and review condition data with County staff to confirm the accuracy of the data collection and interpretation protocols. Our project manager hosted the condition data review on September 19, 2023, where he reviewed site conditions with County staff, received their feedback, and incorporated it into the pavement data processing.

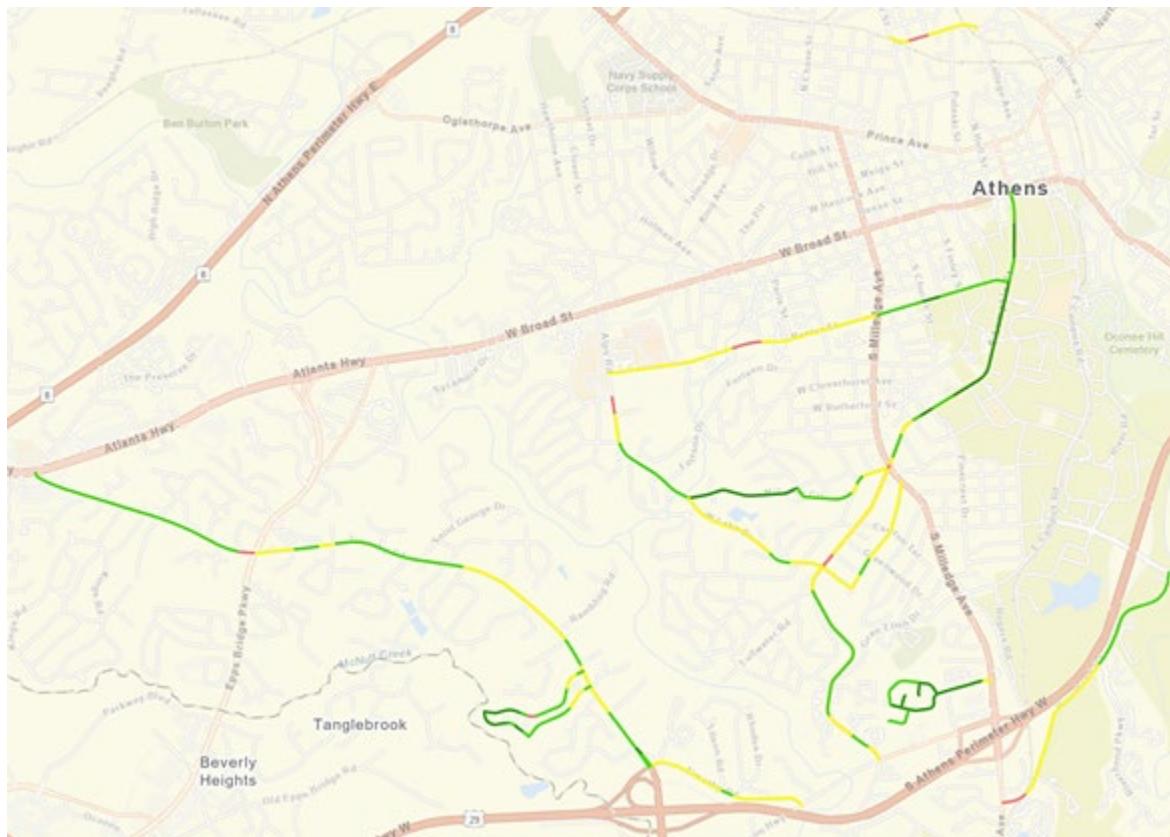


Figure 3. Field pilot pavement sections for PCI data review.

2.4 Pavement Condition Data Processing

After the County's review of the field pilot sections data, we processed the pavement conditions data using the AI pavement rating tool Road Technical Rating Intelligence Program (TRIP™). At this stage, major data processing tasks also occurred, such as generating right-of-way and pavement image streams; calculating profile, roughness and rutting; and detecting cracks, lane markings,

man-made objects, and other distress. The automated crack analysis detected cracks that were then overlaid on the pavement images and offset to assist with the verification of the detected cracks (Figure 4). The severity levels were identified based on the defined limits (ASTM D6433) and verified for resolution through visual quality control checks of image files. The pavement distress (type, severity, and extent) data were loaded into Pavement Express™ pavement management software to calculate PCI values for each pavement section. Each section was rated based on the PCI scale of 0 to 100, where 0 represents a failed roadway condition, and 100 represents an excellent/new roadway condition.

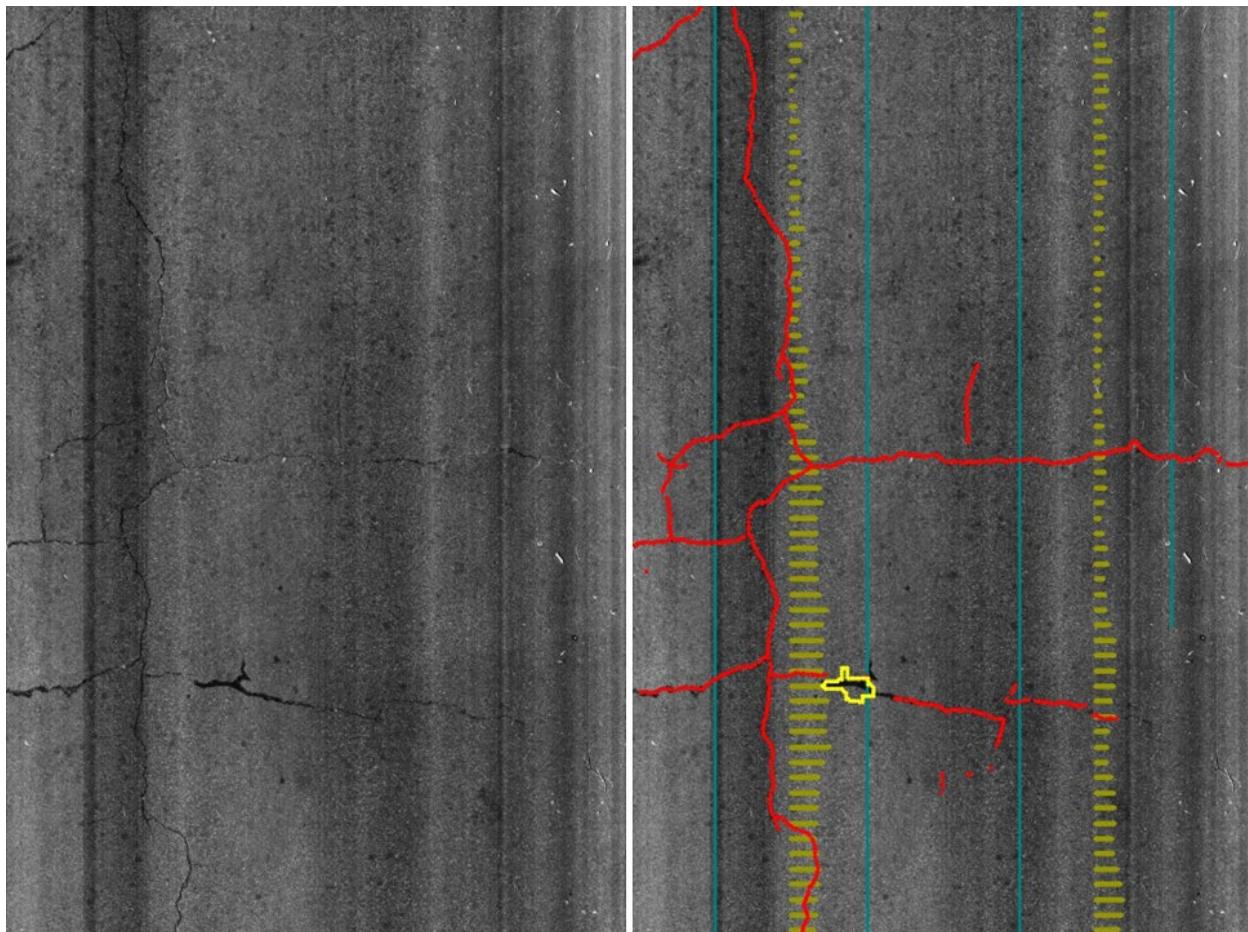


Figure 4. Pavement distress detection process.

An experienced pavement inspector performed quality control (QC) to confirm the distress and severity of the pavement condition data collected by the automated technology. This manual quality review was performed in accordance with the principles of the ASTM D6433 standard, using the LCMS pavement images gathered during the collection with the distresses superimposed and color-coded, such as what can be seen in the corresponding image.

Ten percent of the surveyed sections were subjected to field verification and an internal quality assurance survey by a surveyor. After completion of the PCI calculation, visual checks were performed to ensure that the PCI values were representative of the surveyed images. Sample pavement surface images with representative PCI values are shown in Figure 5. Larger-size images have been included in Appendix A.



Figure 5. Pavement images with different PCI values.

2.5 Pavement Condition Assessment Analysis

After performing a condition inspection of the study area, the inspection data was imported into the Pavement Express™ software. Based on the pavement condition survey, the weighted average PCI of the County's pavement sections is 68.6, which indicates "good" condition. KCI discussed the results of the PCI survey with the County staff on December 22, 2023. Table 2 shows the pavement lane miles and the number of sections by pavement surface type.

Table 2. Summary of the Athens-Clarke County's pavement surface type.

Surface Type	Lane-Miles	Number of Sections
Asphalt	1285.6	4877
Concrete	2.3	11
Asphalt/Dirt	0.1	1
Cobblestone	0.1	1
Gravel	10.5	31

Figure 6 shows the County's pavement area distribution based on 2023 pavement conditions category. About 2.9% of the pavement network is in 'poor', and 'very poor' condition. About 11.3% of

the network is in 'marginal' condition, whereas about 86% is in 'good', 'very good', and 'excellent' conditions.

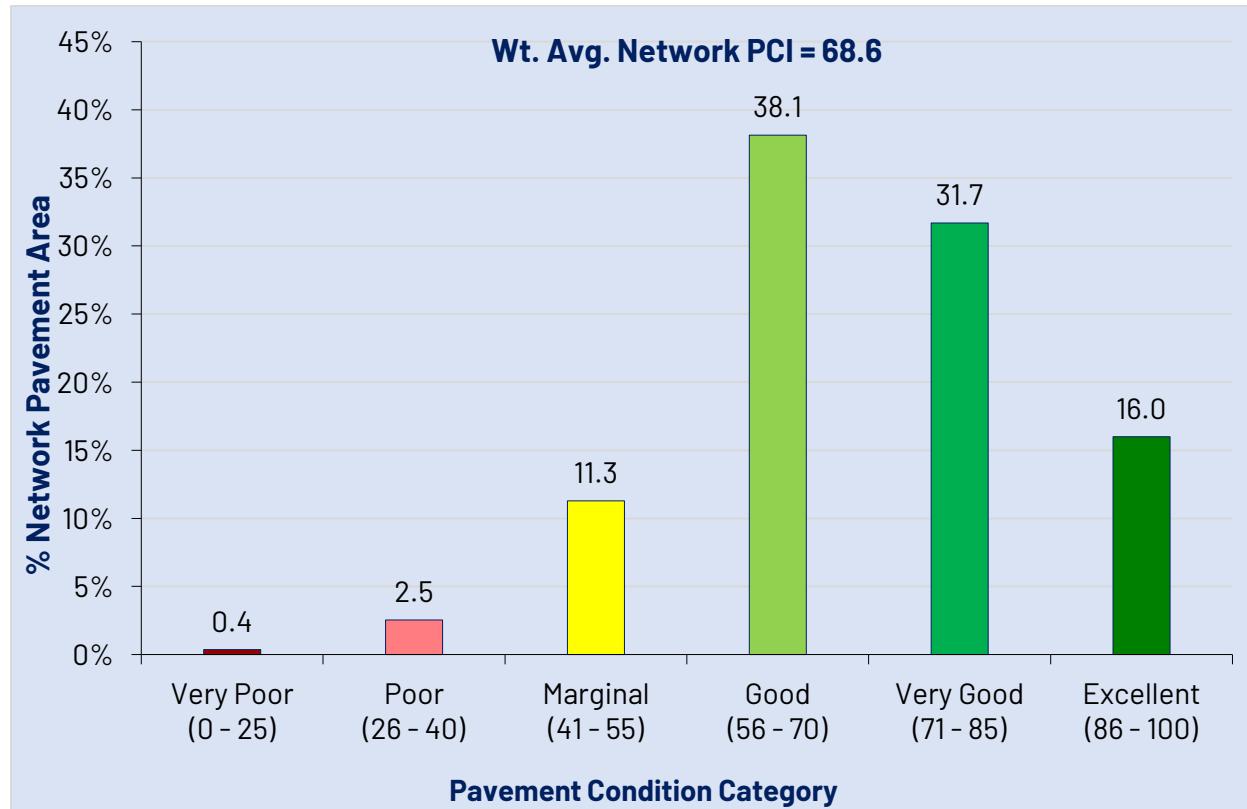


Figure 6. Distribution of the County's pavement area based on the pavement conditions.

Figure 7 shows the distribution of miles of roadways based on pavement conditions. About 18 miles of pavements are in 'poor' and 'very poor' conditions with a PCI range of 0-25 and 26-40, respectively. At this condition level, pavement requires major rehabilitation, such as mill and inlay, full-depth reclamation (FDR), reconstruction, etc. About 70 miles of pavement are in 'marginal' condition with a PCI range of 41-55, requiring minor or major rehabilitation, depending on the existing distress and traffic level. About 228 miles of pavement are in good condition with a PCI range of 56-70. These sections will require localized maintenance, such as crack sealing and patching, to prevent further deterioration and maintain the pavement in good condition, ultimately extending its service life. A total of 202 miles of pavement are in 'very good' condition with a PCI range of 71-85. These pavement sections could be good candidates for applying pavement preservation techniques such as microsurfacing, depending on the existing conditions. Microsurfacing typically adds another 6-7 years of service life to the pavement. Approximately 106 miles of pavement are in 'excellent' condition with a Pavement Condition Index (PCI) range of 86-100. These are newly placed surfaces from the last few years. These sections may have some random minor cracks and are planned to have crack seals applied after the new surface is placed.

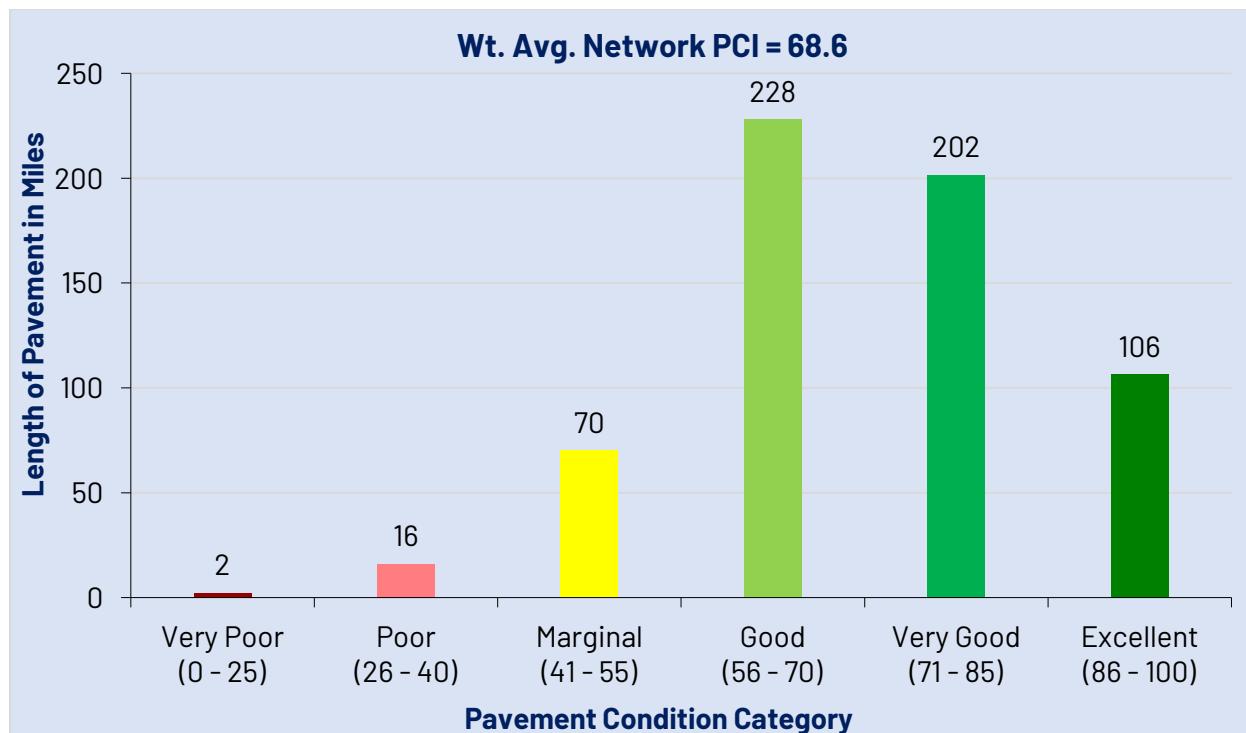


Figure 7. Distribution of the County's pavement lane miles based on the pavement conditions.

Figure 8 shows the County's average pavement condition based on the roadway functional classification (i.e., principal arterial, minor arterial, major collector, and local). On average, the minor arterial streets are in 'very good' condition, while principal arterial, major collector, and local road pavements are in 'good' condition, with average PCIs of 72.5, 62.7, 68.8 and 69.2, respectively.

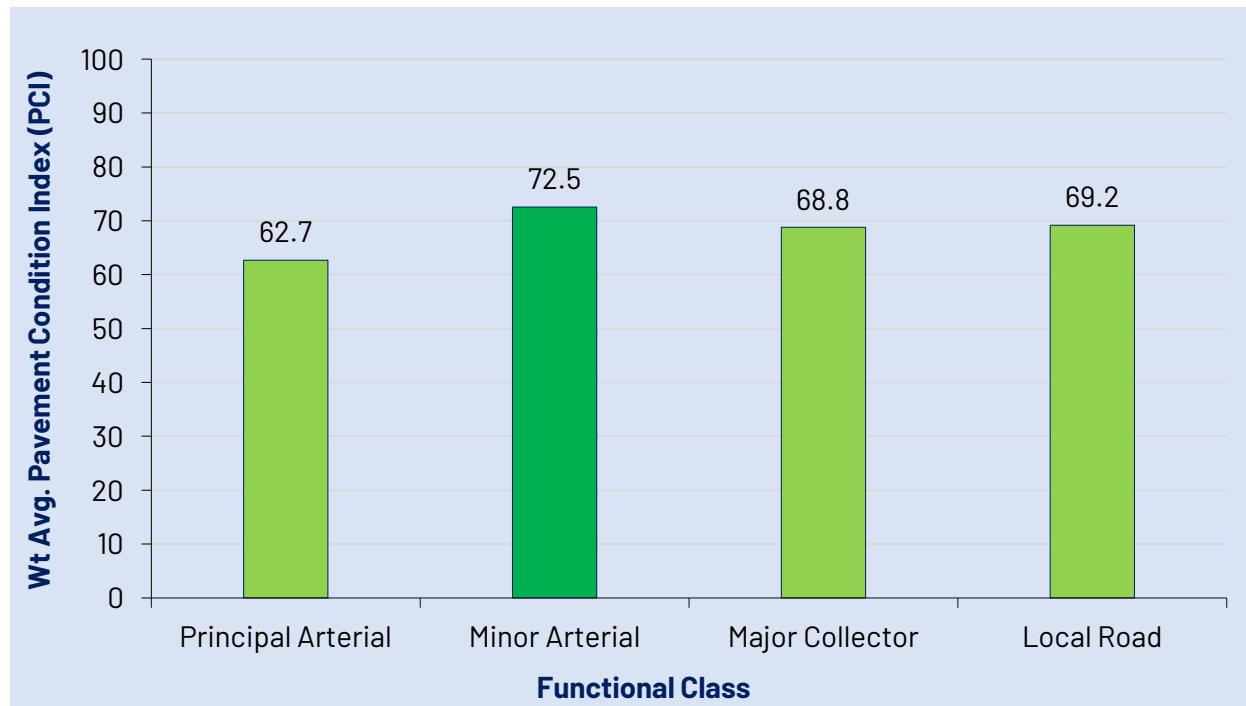


Figure 8. Average Pavement Condition Index based on functional class.

Figure 9, Figure 10, and Figure 11 show the detailed distribution of pavement conditions for arterial, collector, and local roads, respectively. The County has approximately 85 miles of arterial streets in its network. Of these, around 1 mile is in 'marginal' condition, while 84 miles are in 'good,' 'very good,' and 'excellent' conditions.

The County's network includes approximately 80 miles of collector streets. About 4 miles of these are in 'marginal' condition, while 76 miles are in 'good', 'very good', and 'excellent' conditions.

The network includes approximately 454 miles of local streets. About 18 miles of these are in 'poor' and 'very poor' condition, while roughly 64 miles are in 'marginal' condition. About 163 miles of the streets are in 'good' condition, and around 119 miles are in 'very good' condition, and about 90 miles are in 'excellent' condition.

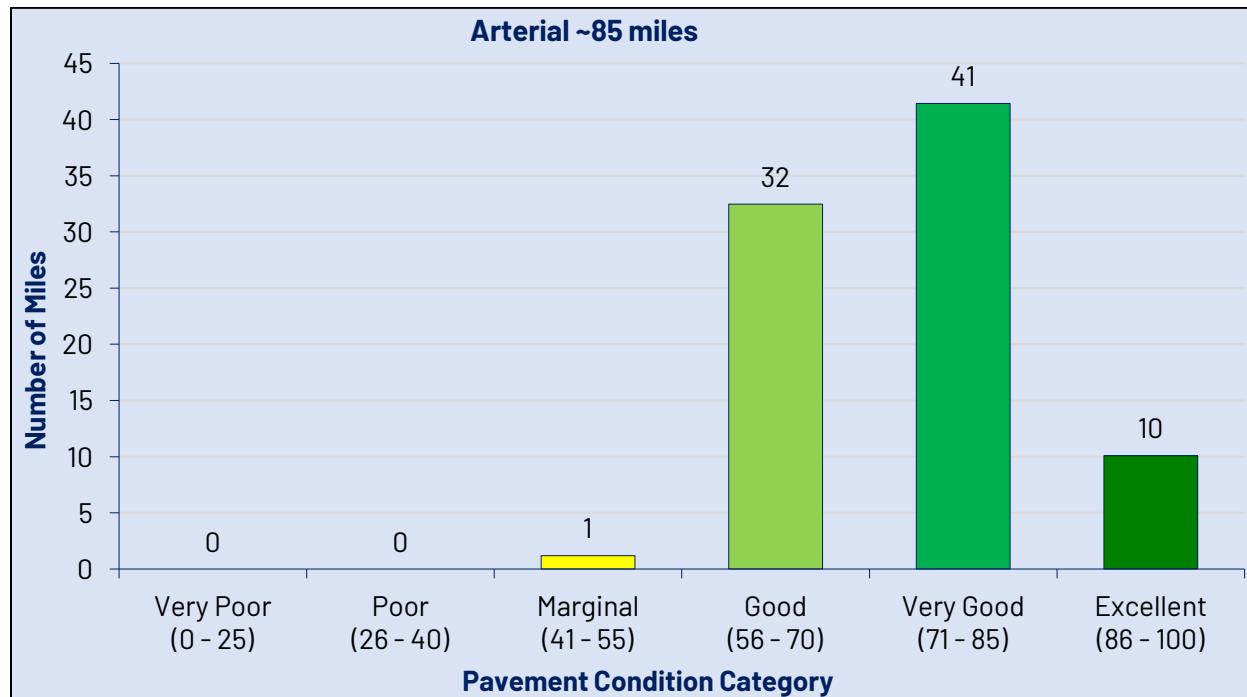


Figure 9. Pavement condition distribution of arterial streets.

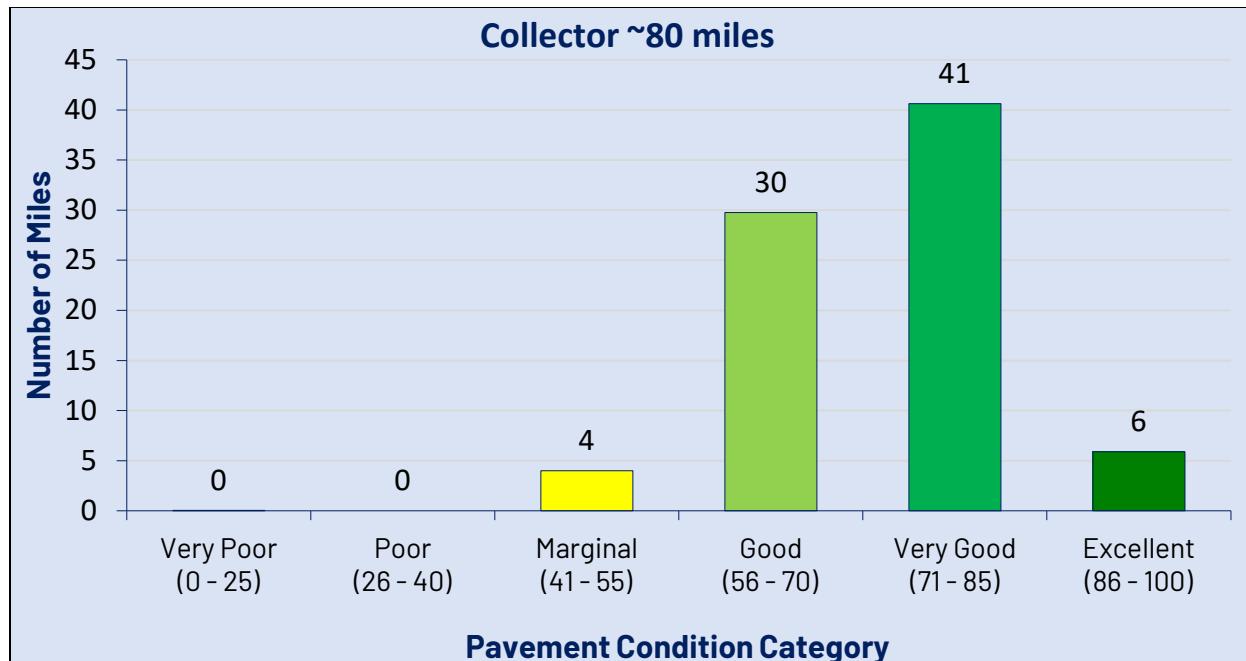


Figure 10. Pavement condition distribution of collector streets.

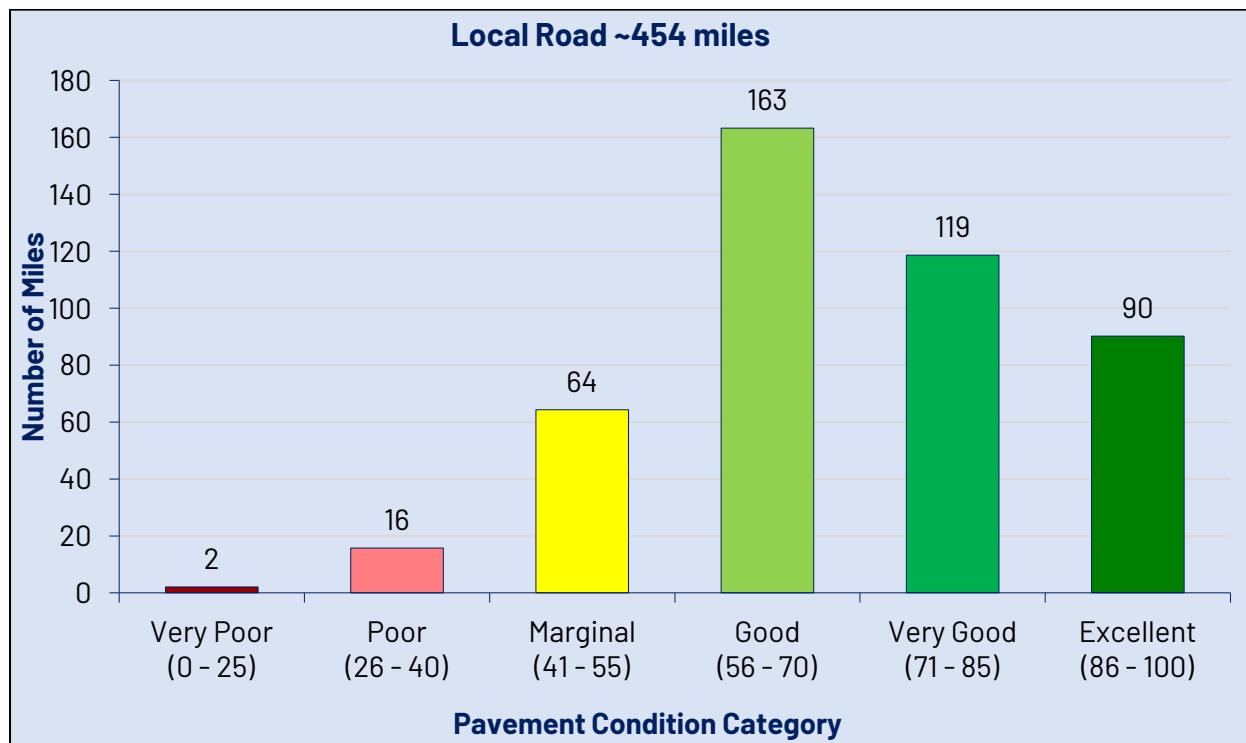


Figure 11. Pavement condition distribution of local streets.

The GIS map with pavement conditions for individual segments is shown in Figure 12. A larger version of this map has been provided in Appendix A.

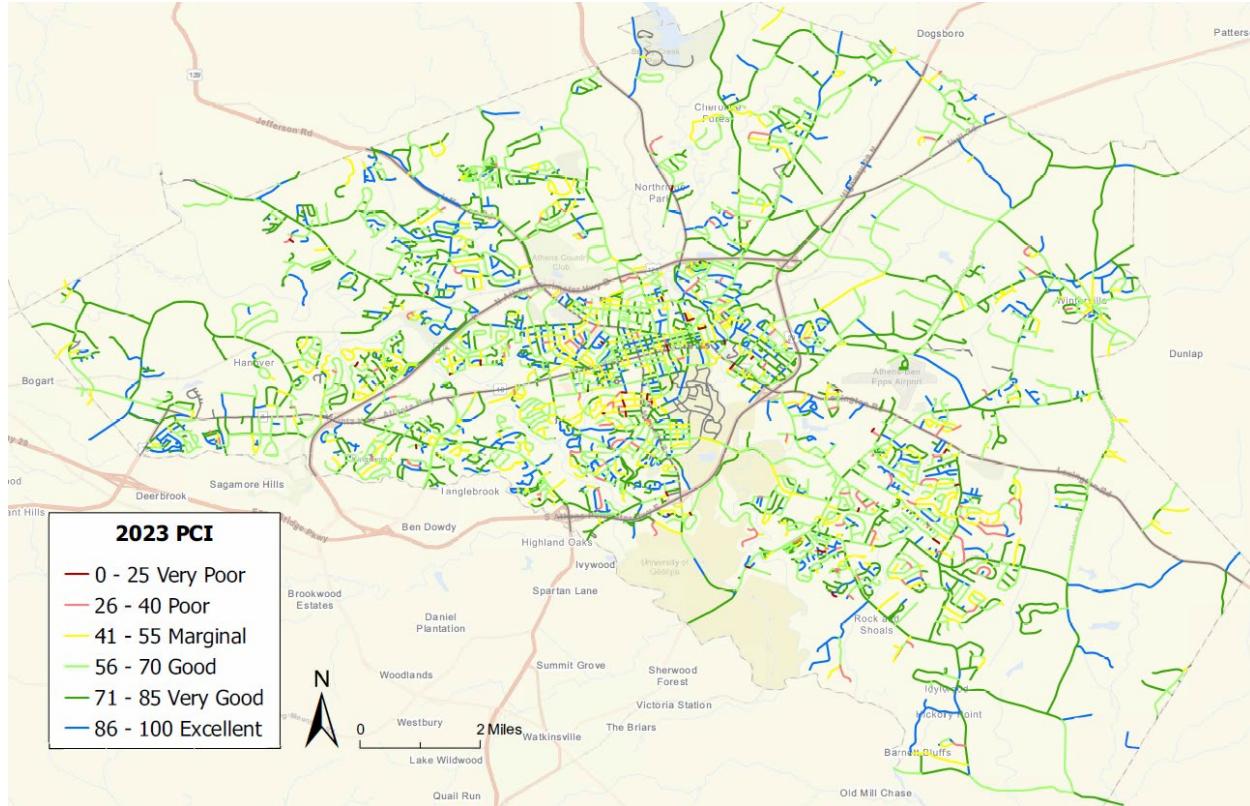


Figure 12. County's Pavement Condition Index (PCI) map based on the July 2023 survey.

The ride quality of street sections is measured using the International Roughness Index (IRI), which is a standard scale. The IRI measurement follows AASHTO R 43-07 and ASTM E950. The IRI indexes are obtained from longitudinal road profiles and processed at 52.8-foot intervals (0.010/mi) using a quarter-car model. KCI uses a three-laser surface profiling system with infrared lasers and a precision accelerometer to evaluate pavement smoothness. The reported values in the centerline are in units of inch/mile. It's important to note that IRI indexes may spike, leading to inaccurate data in sections of roads with slow speeds (below 20 mph).

The county network's weighted average IRI value is 308.8 inches/mile, which indicates that the network is in 'marginal' condition in terms of pavement roughness. Table 3 summarizes the County's pavement roughness conditions.

Table 3. Summary of County's pavement ride quality/roughness

Roughness Category	IRI Range	Number of Miles
Smooth	<200	179
Marginal	200 - 400	273
Rough	>400	171

Figure 13 shows on the next page the County's pavement roughness categories based on the network area. Notably, approximately 29% of the roads are classified as 'smooth,' while 44% are categorized as 'marginal' and 27% are categorized as 'rough'.

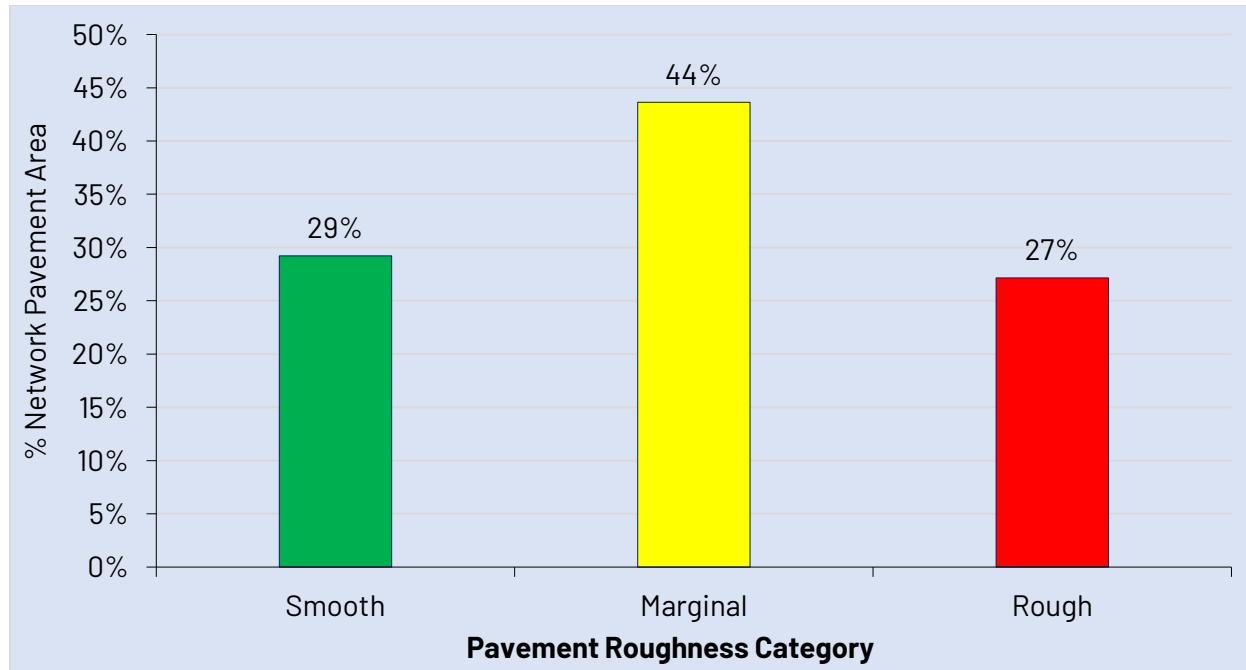


Figure 13. County's pavement ride quality/roughness conditions.

Figure 14 shows the County's pavement ride quality/roughness based on the May 2023 survey.

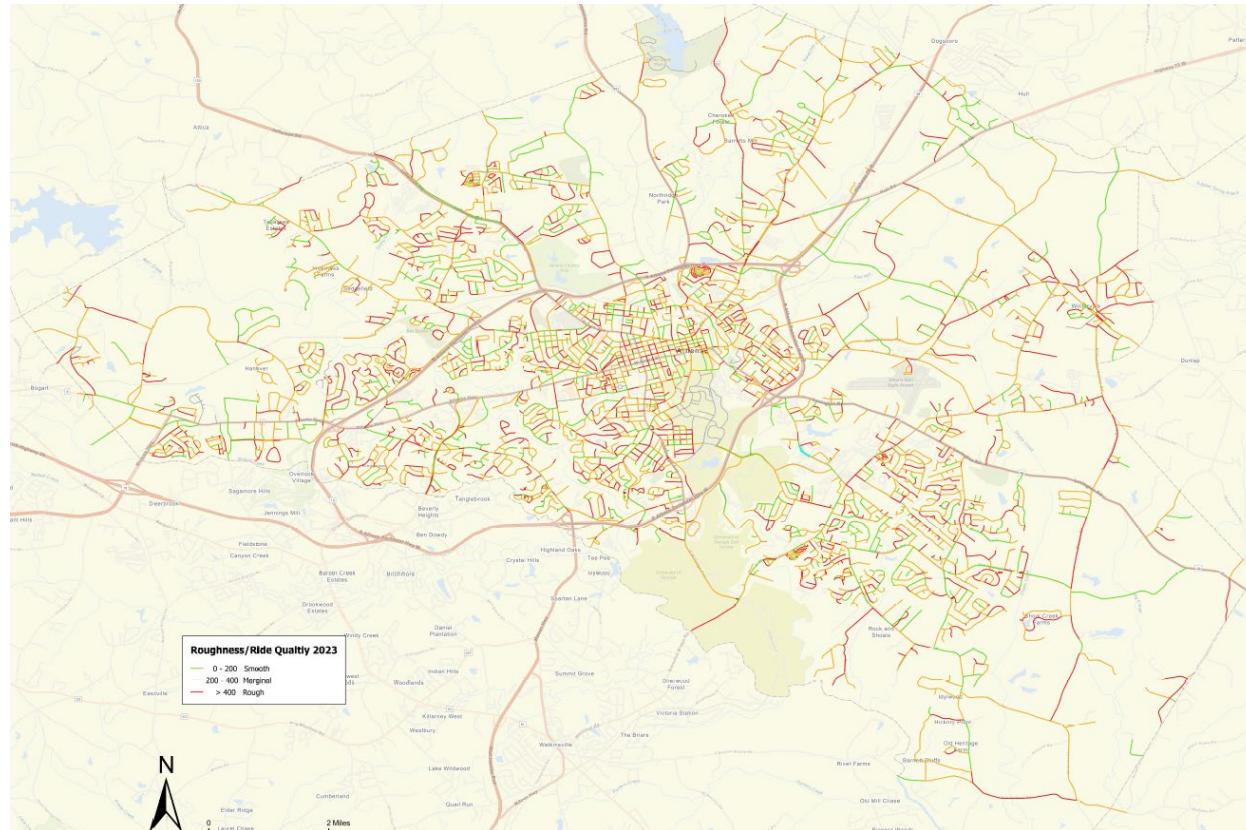


Figure 14. County's pavement ride quality/roughness conditions map.

3 Ground Penetrating Radar (GPR) Data Collection & Processing

KCI contracted the University of Georgia (UGA) pavement research group to conduct a Ground Penetrating Radar (GPR) evaluation of an approximately 200-mile stretch (slow lane only for both directions). The scanned sections are based on the ArcGIS map given to the team.

The main purpose of collecting and processing GPR data is to provide the County with an estimation for pavement layer thickness, based on the scanned GPR data of the project sites. The GPR data was collected in the slow lane using the UGA-GPR van (Figure 15) with a 2.0-GHz air-coupled antenna during October 2023. The project sites for GPR scans are shown in Figure 16. Subsurface images were collected along the test section during the scan to provide continuous data analysis. During the GPR scan, the GPS coordinates were also collected to identify thickness information precisely.



Figure 15. GPR data collection vehicle.

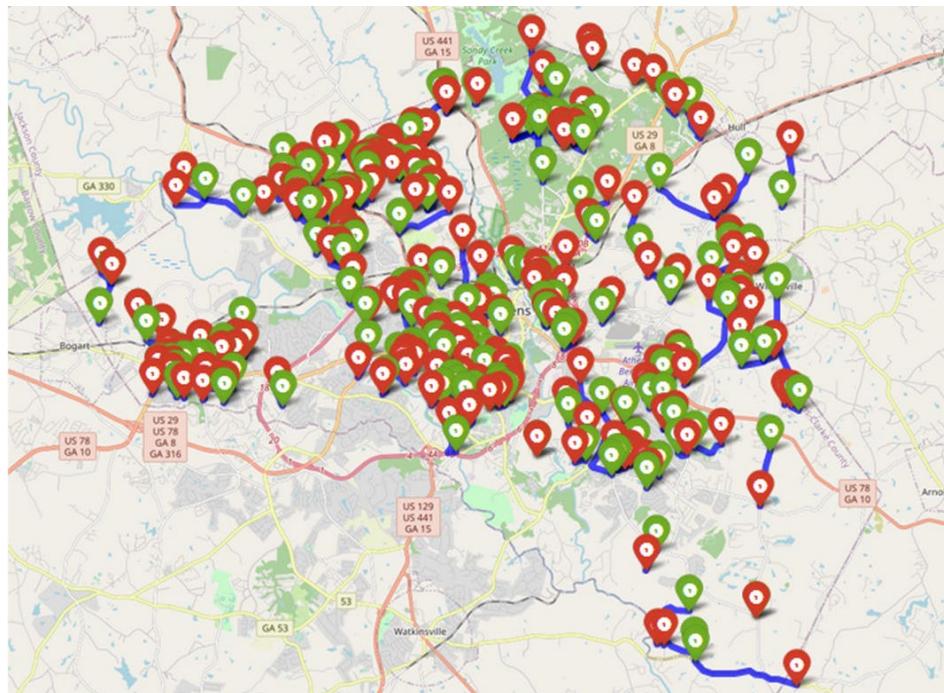


Figure 16. GPR data collection project sites.

3.1 GPR Data Collection

In the evaluation carried out within Athens-Clarke County, an extensive examination was undertaken on 191 road sections to assess the existing state of the asphalt pavement layer. This comprehensive survey encompassed a distance exceeding 200 miles. The specific locations of these sections, along with their corresponding initial and final reference points, have been documented and are presented in Table 1 provided by KCI. The information gathered through this study serves as vital data for future pavement analysis and facilitates informed decision-making in the context of maintenance planning.

Scanning work conducted in this study was carried out using advanced and specialized equipment, specifically the 2 GHz antenna and the SIR-30 data collection system, both from Geophysical Survey Systems, Inc. (GSSI). This combination of hardware was selected for its effectiveness in providing high-resolution subsurface images, for accurate pavement analysis. The specifics of the data collection parameters are outlined in Table 4, which details the technical settings used during the GPR scanning process.

Table 4. GPR data collection parameters.

Scans / Sec	100
Samps / Scan	256
Bits / Sample	32
Antenna Type	42000S
Range (ns)	20
Depth (in)	59.06

Elaborating on each of these parameters:

- Scans per Second (Scans / Sec): Set at 100 scans per second, this parameter indicates the frequency at which the GPR equipment captures subsurface data.
- Samples per Scan (Samps / Scan): At 256 samples per scan, this setting defines the number of data points collected in each scan.
- Bits per Sample (Bits / Sample): The 32 bits per sample setting specifies the data resolution, indicating the amount of detail captured in each sample.
- Antenna Type: The 42000S antenna model from GSSI is used, which is specifically designed for high-resolution GPR scanning. This antenna type is suitable for detecting and imaging shallow subsurface features, making it ideal for pavement analysis.
- Range (ns): The range, set at 20 nanoseconds (ns), determines the depth of the electromagnetic pulse penetration. This depth range is critical for capturing the full profile of the pavement layers, down to the sub-grade.

These carefully calibrated parameters are crucial for acquiring accurate and high-resolution GPR data. The data thus obtained provides a comprehensive view of the pavement structure, enabling precise assessments of its condition.

3.2 GPR Data Processing

The comprehensive analysis of the Ground Penetrating Radar (GPR) data has provided valuable insights into the structural composition of the pavement sections under investigation. The

findings from this analysis are detailed below, with a particular focus on the layered structure of the pavement and the visualization techniques used to interpret the subsurface data.

- **Layer Identification and Composition:** The GPR data analysis has successfully identified four distinct layers within the pavement structure. These are:

- Asphalt Overlay: This is the uppermost layer.
- Old Asphalt Layer: This older layer is situated directly beneath the overlay and represents the original pavement surface.
- Sub-base Layer: Found beneath the asphalt layers.
- Sub-grade Layer: The bottommost layer represents natural earth material.

Since the scope of this Stage 1 report, is to identify the thickness and change in thickness for different sections, the report focuses on top asphalt layers.

- **Visualization of Subsurface Layers:**

- Creation of GPR Images: To aid in the visual interpretation of these subsurface layers, GPR images have been generated. These images are instrumental in providing a clearer picture of the variations in layer thicknesses and the overall integrity of the pavement structure.
- Location and Accessibility of GPR Images: The GPR images, specifically showcasing the subsurface layers of Amethyst Drive, are exemplified in Figure 17. These images serve as a valuable tool for visualizing the layer stratification and identifying areas of concern. All the GPR images corresponding to each scanned section of the pavement are methodically organized and available in the attached folder. This repository of images can be referred to for a detailed examination of the pavement's subsurface characteristics.

The findings from the GPR data analysis are integral in guiding future maintenance and repair strategies. They offer a deeper understanding of the pavement's current condition and lay the groundwork for targeted interventions aimed at prolonging the pavement's service life and ensuring its structural integrity. The findings from this analysis are detailed below, with a particular focus on the layered structure of the pavement and the visualization techniques used to interpret the subsurface data. Figure 17 shows a GPR scan of Amethyst Dr.

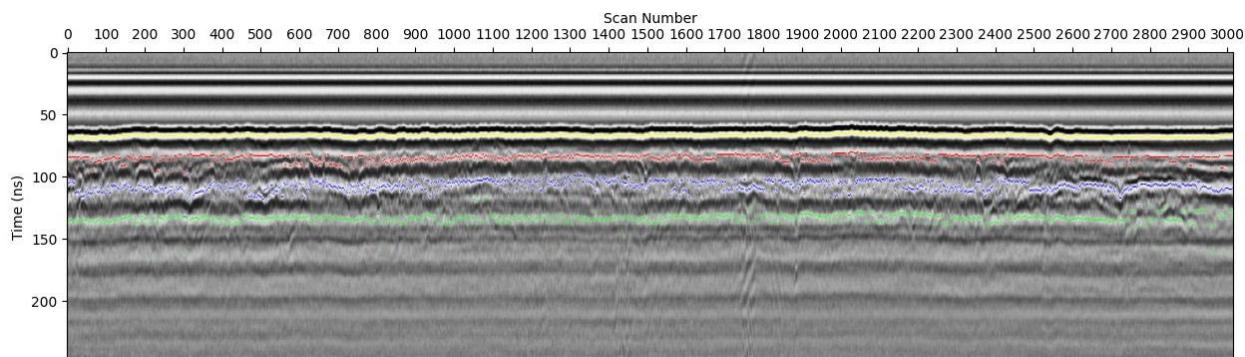


Figure 17. Amethyst Dr, GPR image.

Also, a heat map representing change in asphalt layers thicknesses is presented in Figure 18 and Figure 19.

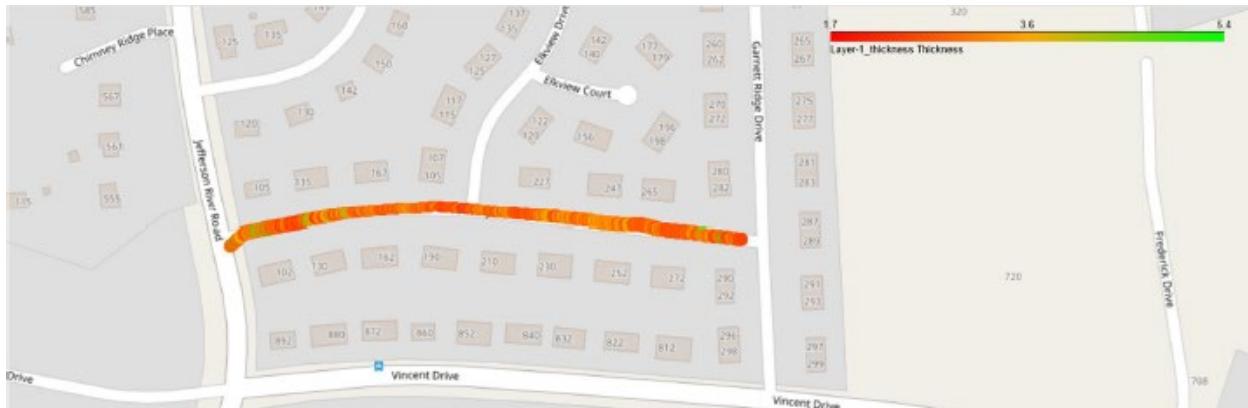


Figure 18. Amethyst Dr, Layer 1 thickness heat map

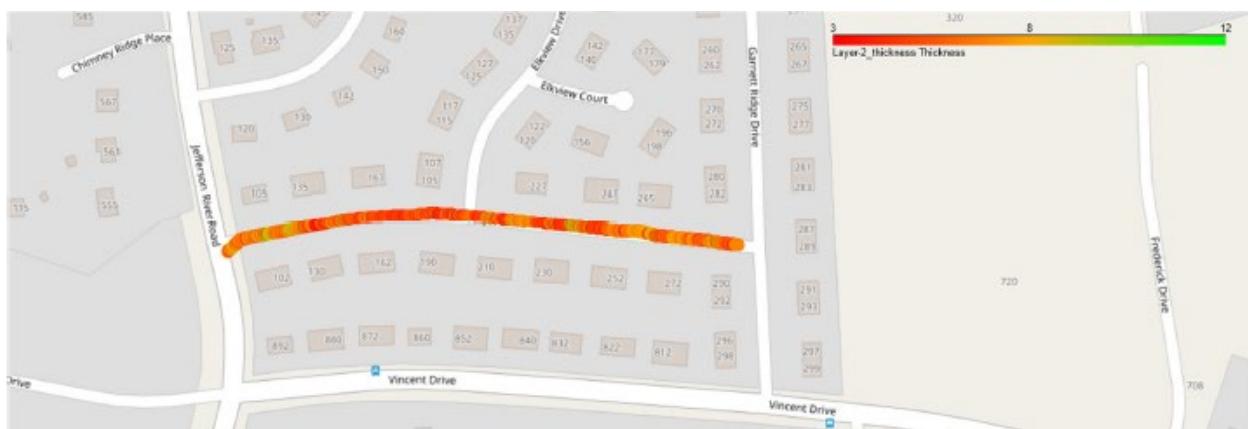


Figure 19. Amethyst Dr, Layer 2 thickness heat map

3.3 Proposed Coring Locations

The analysis of the (GPR) data indicates that certain pavement sections exhibit variations in layer thickness that requires further investigation. A list of coring locations has been provided in the Appendix A. Specifically, sections where the overlay layer is less than 2 inches thick, as well as sections where the cumulative thickness of the asphalt—comprising both the overlay and the underlying old asphalt layer—is less than 6 inches, should be prioritized for coring. This additional field testing is essential to verify the GPR analysis against actual field measurements and to ensure the integrity of the pavement structure.

4 Pavement Management Analysis

4.1 Pavement Management Software Overview

KCI used Pavement Express™, a pavement management software, to implement a PMS for Athens-Clarke County. Pavement Express is a pavement management system designed to help users effectively manage its pavement network. It stores, retrieves, and processes pavement-related condition and inventory data, allowing users to analyze the current condition, future performance, and expected financial needs of the network. Users are able to:

- View network inventory spatially and be able to see the different classifications and attributes of the network
- See and update the current condition survey
- Model various real-world scenarios that account for multiple constraints. For example, user can compare scenarios in which user maintains a specific level of service for different classes of roadway segments, pavement surface types, or a particular jurisdiction while adhering to a specified budget amount, either at the network level or broken down for each constraint. Users can model all these constraints in a single scenario using the system's advanced optimization engine to produce a work plan with a set of treatments that achieve a defined objective
- Use the interactive dashboards and reports in Pavement Express to communicate the historical, current, and future state of your pavement network based on key performance measures that user designates or creates

4.2 Pavement Performance Models

A PMS is only useful for making decisions if performance models can be established, validated, and relied upon to forecast pavement conditions into the future. A pavement performance model is developed based on the date of construction for new pavement and the date of resurfacing for an overlay or mill and overlay, the types and thicknesses of pavement materials, the traffic level, and the pavement condition. The pavement performance model becomes more accurate with multiple pavement condition ratings as the model gets calibrated and adjusted to match the conditions in a pavement's life cycle.

The Performance Model Setup module in Pavement Express™ helps identify and group pavements of similar construction subjected to similar traffic, weather, and other factors affecting pavement performance. The pavement condition historical data are used to build a model that can accurately predict the future performance of a group of pavements with similar attributes.

Three pavement performance models were developed based on the asphalt-surfaced pavement owned and maintained by the County. The pavement performance model for the County was developed based on the available age data. The reliability of the pavement performance model is expected to increase with future pavement inspection and age data. Figure 20 shows the pavement conditions deterioration model for arterial, collector, and local road pavements of the County.

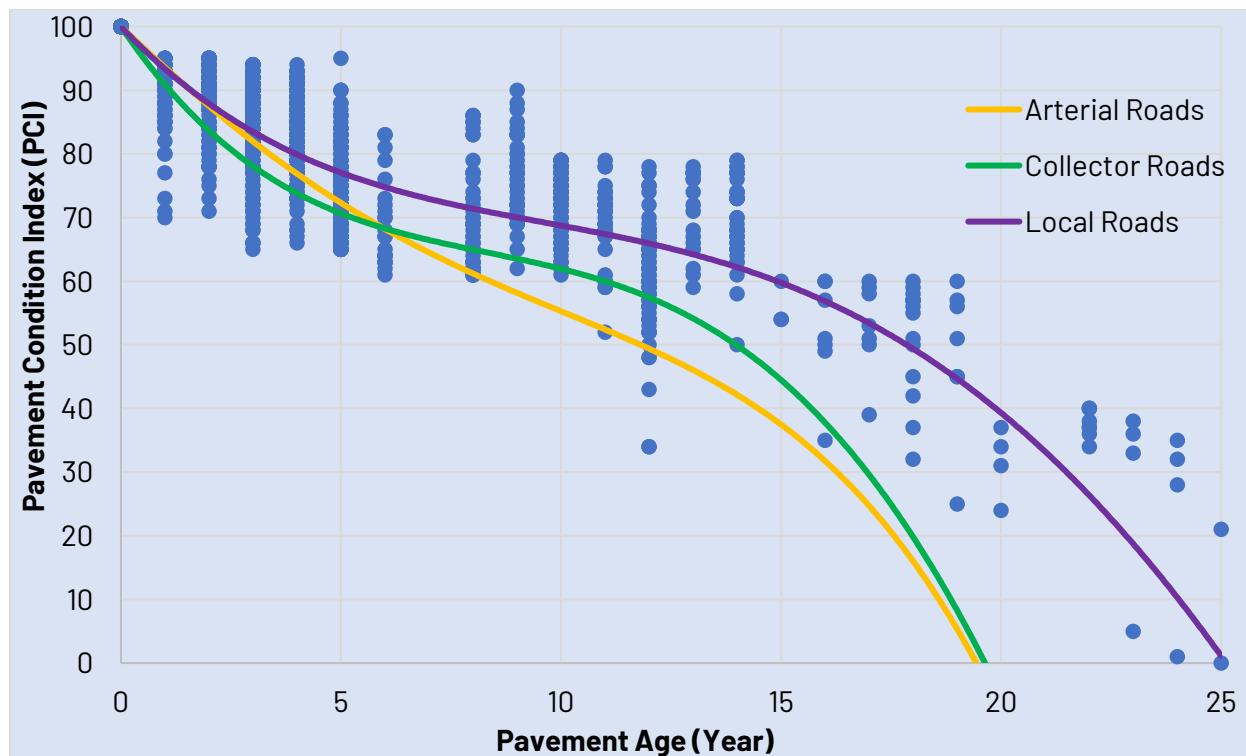


Figure 20. Pavement performance model for the County's arterial streets.

4.3 Treatment Decision Tree

Pavement Management refers to applying the right treatment to the right pavement, at just the right time for optimal roadway maintenance planning. Three types of maintenance and repair works are recommended for effectively maintaining the roadways and optimizing the budget.

- Preventive Maintenance: Crack sealing, patching, etc., to slow down the rate of deterioration
- Pavement Preservation: A proactive method to keep good roads good. It can be compared to changing engine oil periodically to keep it in good condition and extend service life. Microsurface is widely used as a pavement preservation treatment.
- Major Rehabilitation: Mill & inlay, full-depth reclamation, and reconstruction

KCI worked with the County staff to evaluate the current maintenance practices. Currently, the County is applying crack seal and patching as localized preventive maintenance and 1.5" mill and inlay as major rehabilitation when PCI below is under 40. KCI has recommended pavement treatment types based on pavement conditions and the functional class of the streets. Based on the pavement rehabilitation techniques currently used and discussion with the County, KCI developed a treatment decision tree that defines when a treatment will be performed based on PCI values and functional class (Figure 21 on Page 23). The Decision Tree module is used to designate sections to receive specific M&R work, including:

- Localized Maintenance - Preventive/Routine Maintenance
- Pavement Preservation
- Major Rehabilitation

The decision tree will first examine segments with low PCI ($PCI \leq 55$) and determine the best approach/treatment to improve the roadway condition. If there is a localized deterioration present that can be addressed via limited deep patching treatment, the road segment is placed in the preventative maintenance list for deep patching. Otherwise, the road and adjoining segments will be examined under the resurfacing treatment matrix shown on the next page.

Based on Figure 21 on the next page, pavement sections with a PCI above the critical value of 55 are chosen for localized preventive treatments like patching. It is T&PW staff and KCI recommendations to set a different PCI threshold for resurfacing and/or rehabilitation per classifications as follows:

- Local – $PCI \leq 40$
 - These roadways provide access to homes, businesses, and other property
 - Posted speed generally 25 mph (may be greater)
 - Have a low truck and heavy equipment traffic and the low traffic
- Collector – $PCI \leq 48$
 - These roadways link arterials and local roads and perform some of the duties of each.
 - Posted speed typically exceeds 30 MPH
 - May have residents - rural sections
 - Have higher truck traffic and heavy equipment than local roadways.
- Arterials – $PCI \leq 55$
 - These roadways provide mobility so traffic can move from one place to another quickly and safely.
 - Posted speed 40+ MPH
 - Have high traffic with higher truck traffic and movement of goods and services than Collector roadways.

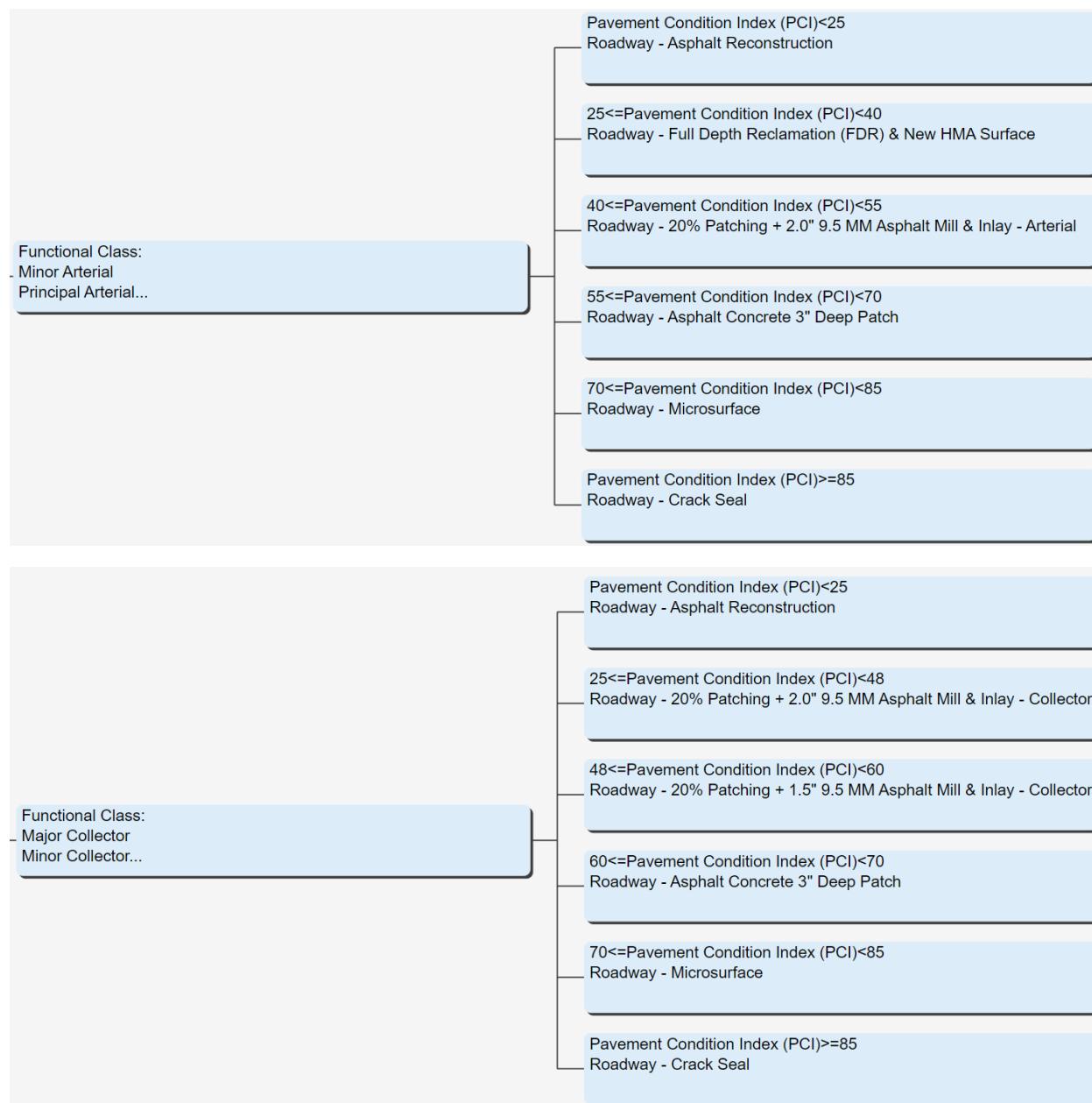
When a roadway segment is below the threshold for resurfacing based on roadway classification, multiple steps are taken to select the preferred treatment:

Existing curb condition and curb reveal: T&PW will build a tailored treatment to re-establish curb reveal. This is projected to build a deficit list of roadways that need full-depth reconstruction (FDR). When a road is put on the FDR list, it will continue to receive patching treatments to maintain an acceptable PCI (above threshold) until funding is adequate to move into FDR work and contracts.

Subsurface conditions: Many ACC roadways lack adequate subsurface compaction, resulting in a short service life for roadway pavement. Roads with failed or inadequate subsurface conditions that do not qualify for traditional resurfacing will be placed on the FDR list of roadways. When a road is put on the FDR list, it will continue to receive patching treatments to maintain an acceptable PCI (above threshold) until funding is adequate to move into FDR work and contracts.

Pavement design based on roadway classification and traffic loading: A systemic design process is needed for selecting the mill and inlay thickness that is based on the subsurface conditions and traffic loading with the intent to re-establish curb reveal and proper drainage in the roadway.

Applying pavement preservation: Microsurfacing is recommended for streets with a PCI greater than or equal to 70 to preserve the pavements that are in satisfactory and good condition. The county's current practice is to apply crack seal four years after paving. Major M&R includes milling, patching and overlays of varying thickness, with a crack interlayer in some cases, or reconstruction for severely deteriorated streets. Notably, the final selection of pavement treatment should be determined based on the recent condition evaluation, existing distress, subgrade, and curb reveal condition. At this PCI level, the pavement should be thoroughly assessed for structural condition, subgrade issues, and curb reveal to determine the appropriate treatment.



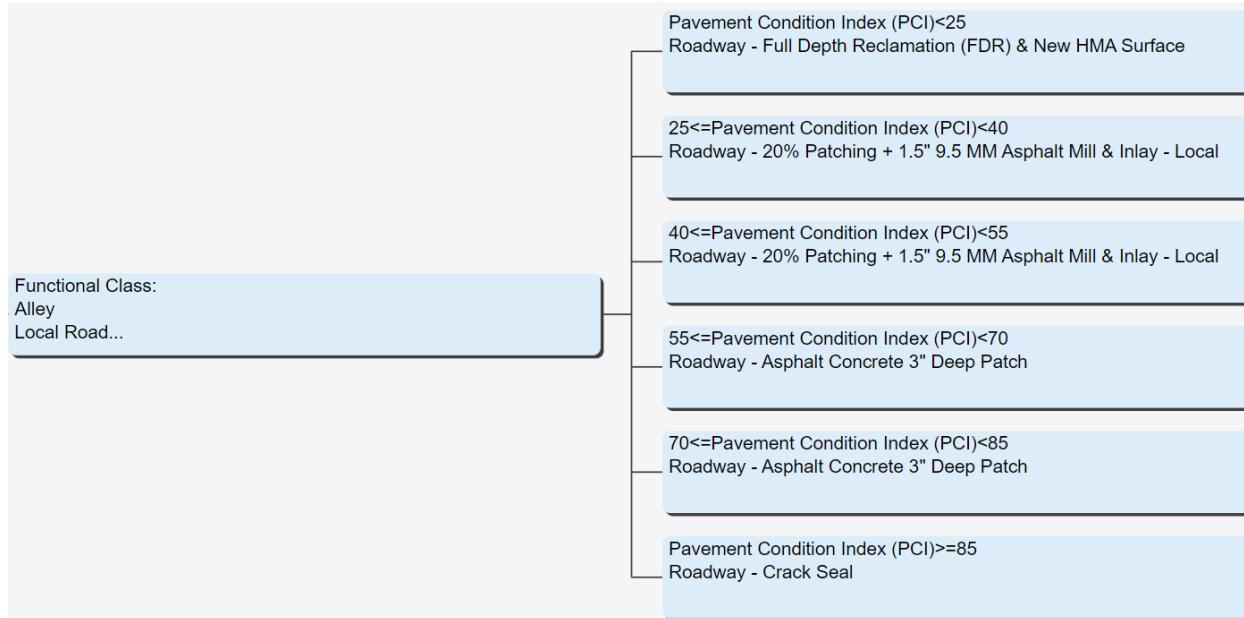


Figure 21. Pavement treatment selection decision tree.

4.4 Unit Costs

KCI determined the unit costs for most treatments listed in Table 5 based on information provided by the County. These costs were discussed with the Athens-Clarke County staff. The unit costs used for *Pavement Express™* analysis for 2024-2028 are shown in Table 5. The table also shows the change in PCI values and pavement age based on the treatment type. The unit costs can be updated based on the available unit price of materials and construction to run the PMS analysis in the future.

Table 5. Treatment unit costs.

Treatment Name	Unit Cost	Units	ΔPCI	Age
Crack Seal	\$0.06	Ft	+5	+1
Asphalt Concrete 3 Deep Patch	\$3.42	Yd	+10	+1
Microsurface	\$3.60	Yd	+18	+1
Single Surface Treatment	\$9.05	Yd	+18	+1
PCC Slab Replacement	\$120.00	SqYd	+30	+1
20% Patching + 1.5 9.5 MM Asphalt Mill & Inlay - Collector	\$42.84	SqYd	100	0
20% Patching + 1.5 9.5 MM Asphalt Mill & Inlay - Local	\$42.84	SqYd	100	0
20% Patching + 2.0 12.5 MM Asphalt Mill & Inlay - Arterial	\$48.84	SqYd	100	0
20% Patching + 2.0 12.5 MM Asphalt Mill & Inlay - Collector	\$48.84	SqYd	100	0
Full Depth Reclamation (FDR) & New HMA Surface	\$60.00	SqYd	100	0
Asphalt Reconstruction	\$65.00	SqYd	100	0
PCC Reconstruction	\$105.00	SqYd	100	0

4.5 Budget Scenario Analysis

Using the pavement deterioration model, treatment applicability, treatment type, and unit costs, we have developed the following five budgets and pavement conditions scenarios to evaluate where the 2024 budget level stands:

- Scenario 1: Eliminate backlogs (Backlog means pavement with PCI value less than 55);
- Scenario 2: Maintain current condition (PCI = 68.6);
- Scenario 3: Increase the funding level to \$13.5M/year from CY2025 PMP Program (\$13.5M/year with \$500K/year for microsurfacing and treatments based on PCI value and functional class, and would include the performance of GPR on all roads);
- Scenario 4: Maintain the 2024 funding level in the CY2024 PMP Program (\$8.0M/year with \$500K/year for Microsurfacing);
- Scenario 5: Maintain the 2024 funding level in the CY2024 PMP Program (\$8.0M/year with current treatment practices);
- Scenario 6: Do nothing (\$0/year).

The outcomes of the budget scenario analysis are presented in Figure 22, illustrating the effect of different funding levels on the average pavement condition of the roadway network. Figure 22 indicates that the 2024 funding level (\$8M/year) with current treatment practice will decrease the average PCI value to 62.5 after five years. However, applying microsurface will increase the PCI value by 1.1 points with the same budget. Increasing the funding level to \$13.5/year from CY2025 will result in overall PCI to 66.9 after five years. Moreover, the County would need to invest \$14.8M/year on maintenance and rehabilitation to maintain the overall 2023 condition, i.e., PCI of 68.6. Allocating a budget to eliminate backlogs where no pavement is in poor condition (PCI<55) would result in an average PCI of 79.1 after five years, requiring an investment of \$45M/year. Conversely, not spending funds on the maintenance and rehabilitation (M&R) program will deteriorate the network to an average PCI of 54.0 after five years.

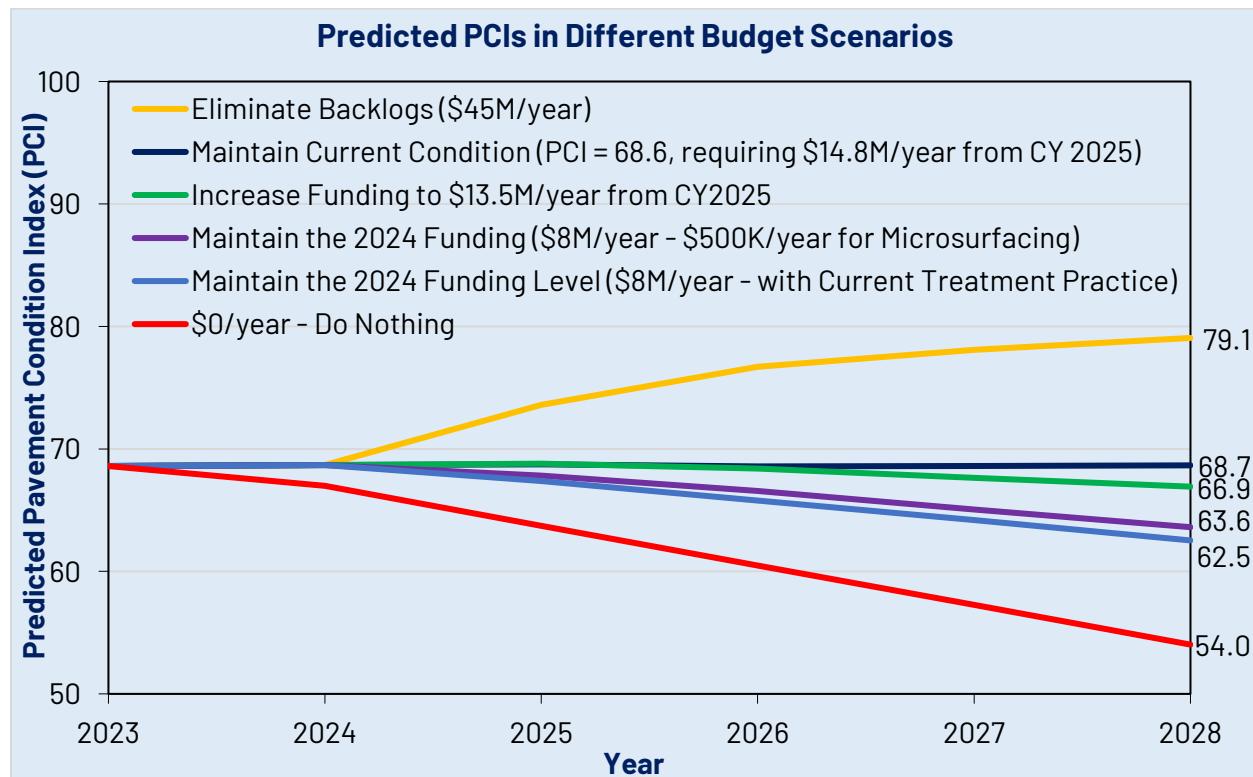


Figure 22. Effect of funding level on County's pavement conditions.

Figure 23 shows the remaining backlogs in terms of centerline miles of major rehabilitations required per year. The backlog grows whenever funding is insufficient to keep up with the rate of street deterioration and the number of street segments in poor or worse condition grows. As more streets fall into this category, rehabilitation costs increase. Based on the 2024 funding with current treatment practice, the backlog would increase to about 172 miles of pavements in poor or worse condition after five years. Increasing funding level to \$13.5M annually from CY2025 would result in a backlog of 136 miles and a PCI of 66.9 after five years. To maintain the 2023 condition (PCI of 68.6), would require \$14.8M annually for the next four years from CY2025, resulting in a backlog of 119 miles. An annual investment of \$45M would be required from CY2025 to eliminate the backlog in five years, resulting in an average PCI of 79.1 and backlog of 3 miles. However, not spending any funds on maintenance and rehabilitation will generate backlog of 223 miles after five years.

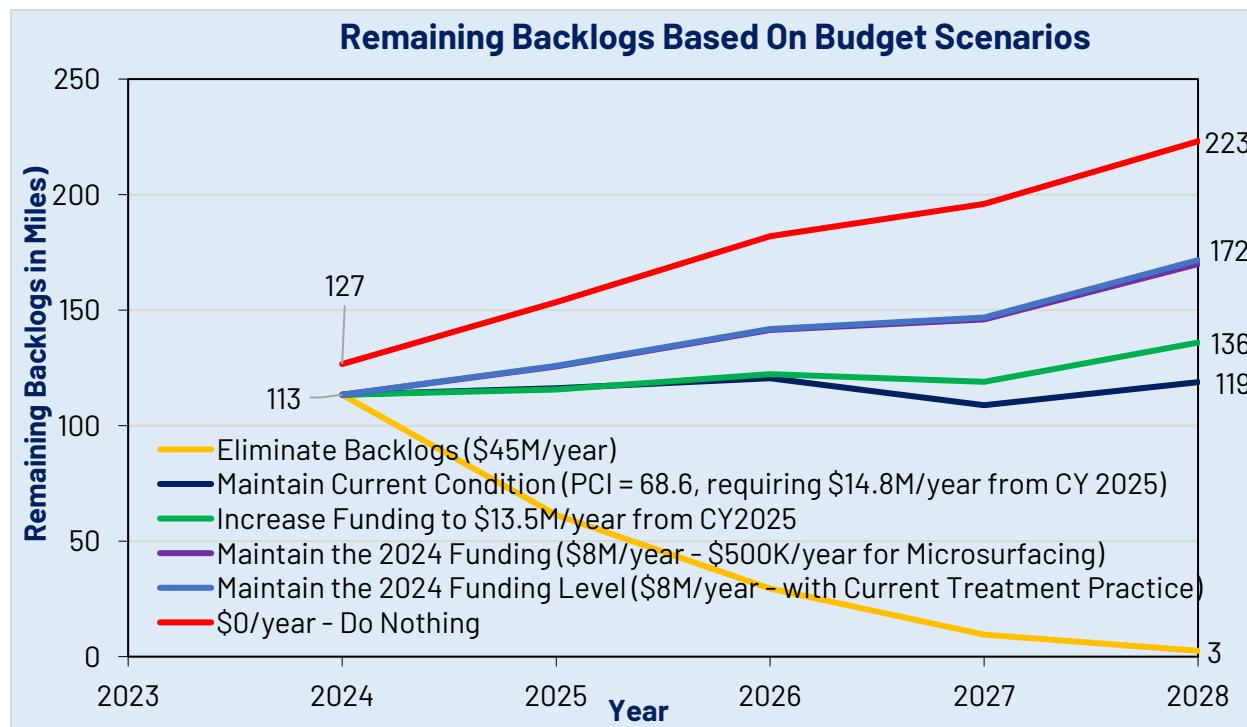


Figure 23. Total unfunded/remaining backlog per year based on budget scenarios.

5 Multi-Year M&R Plan & Approved CY24 PMP Projects

5.1 Localized Distress Maintenance

A maintenance plan for localized distress involves calculating the cost and condition of immediate M&R implementation based on the most recent inspection year. The County applies both crack seal and patching on arterial and collector streets, but only patching on local roads for localized distress maintenance. Also, the County spent about \$1.0M/year for 12.5 mm mix patching in the past several years and \$150 K for crack sealing annually. Based on the treatment decision tree and budget optimization, the Pavement Express™ has selected pavement sections for localized distress maintenance. Appendix A provides the details of the localized distress maintenance plan based on the 2023 condition survey, while Table 6 shows the cost and pavement condition in 2023 as a result of the plan.

Table 6. Summary of the local distress maintenance plan (2024).

Year	Treatment	Quantity	Unit	Number of Sections	Estimated Costs
2024	Patching	117,093	SqFt	2900	\$ 887,567
2024	Crack Seal	96,824	Ft	2692	\$ 145,237

Figure 24 shows 2024 localized distress maintenance plan. This map shows the sections where preventive and safety related maintenance works are required based on the 2023 pavement condition assessment. A larger version of this map has been provided in Appendix A.

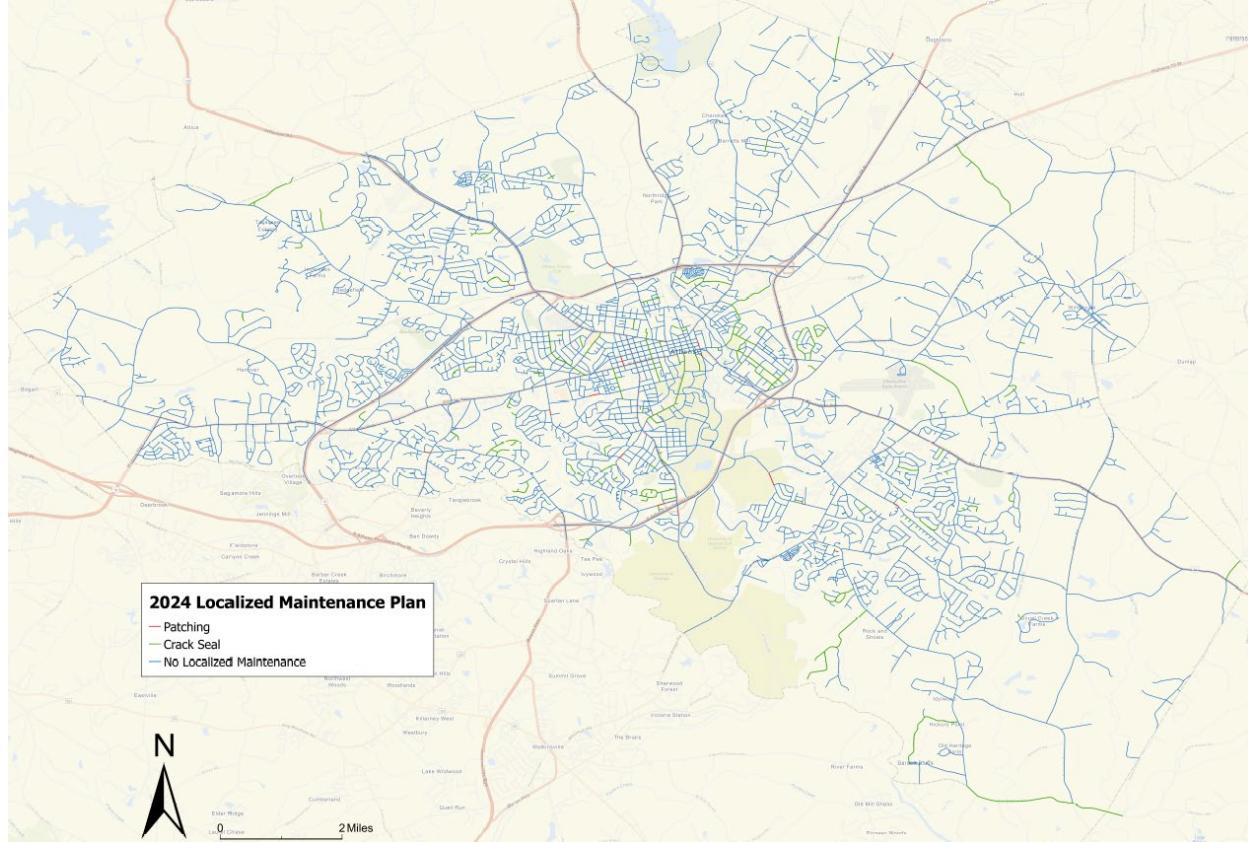


Figure 24. Localized Maintenance Plan 2024.

5.2 Major Rehabilitation

Figure 25 shows the predicted number of miles to undergo Major M&R such as mill & inlay, full-depth reclamation, and reconstruction and the resulting PCI values based on the 2024 budget (\$8M/year).

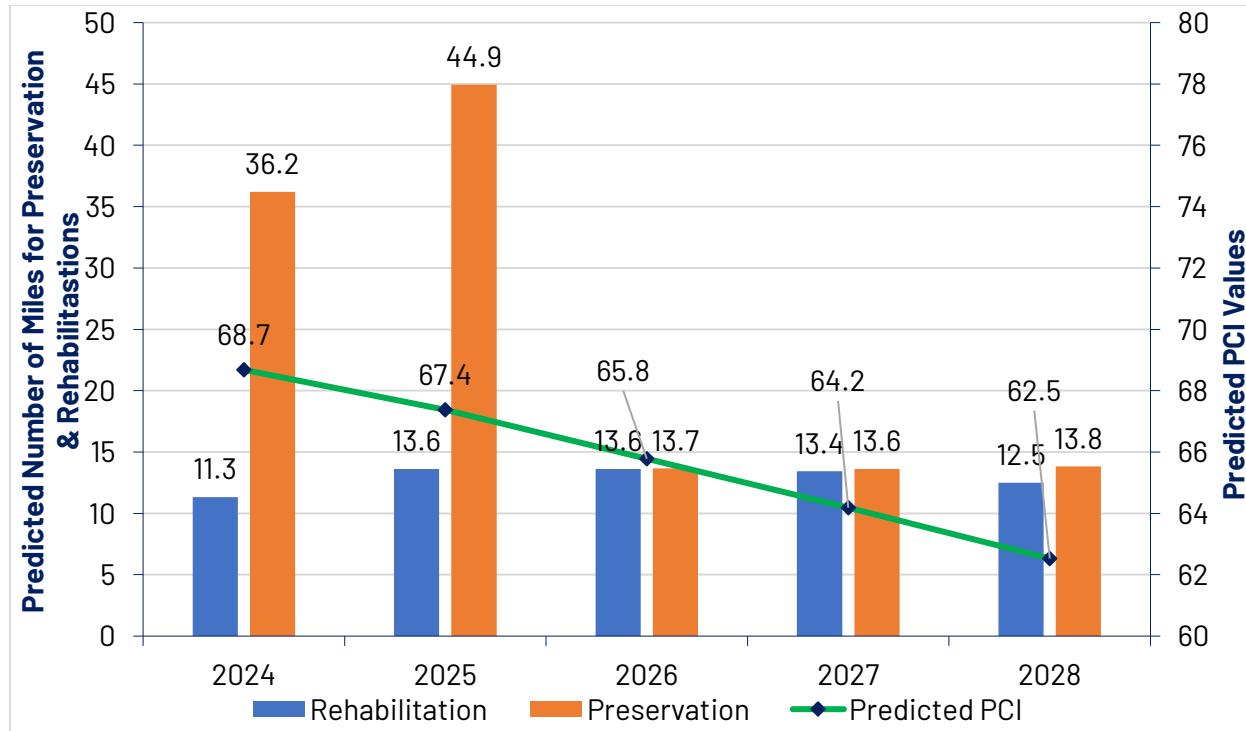


Figure 25. Predicted number of miles that can be resurfaced and resulting average PCI per year based on the 2024 budget.

The selected pavement sections for Major M&R and preservation are shown below in Figure 26 and Figure 27, respectively. A larger version of these maps have been provided in Appendix A.

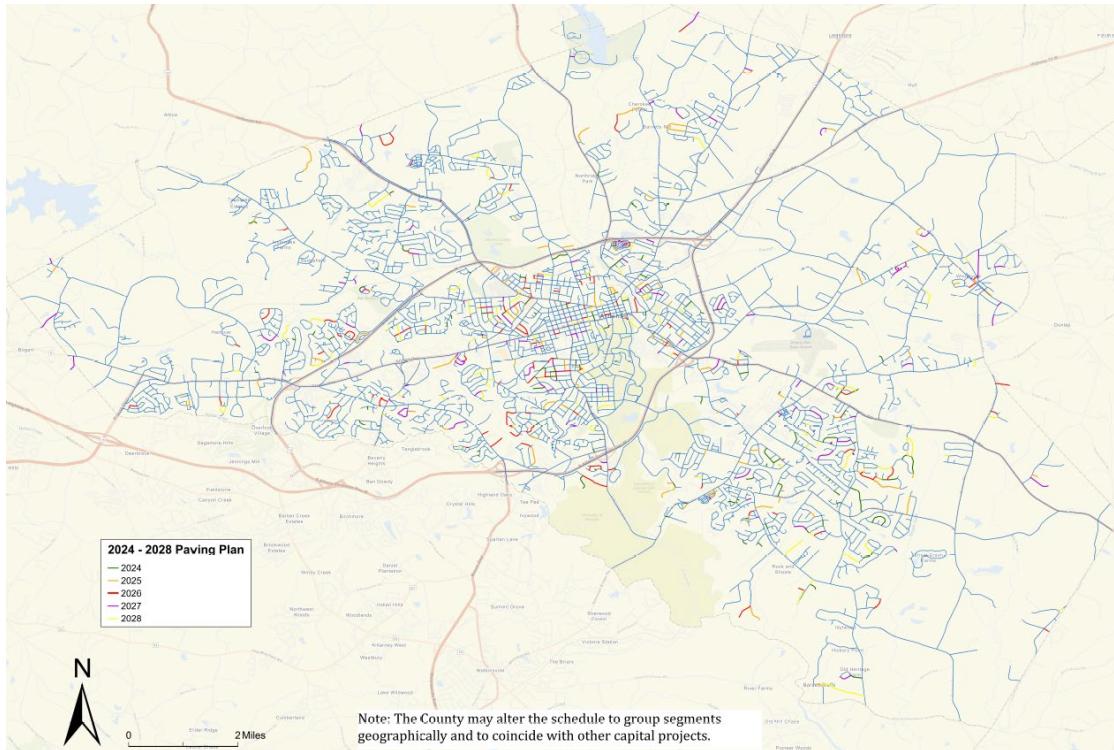


Figure 26. A 5-Year Major M&R plan based on the 2024 budget.

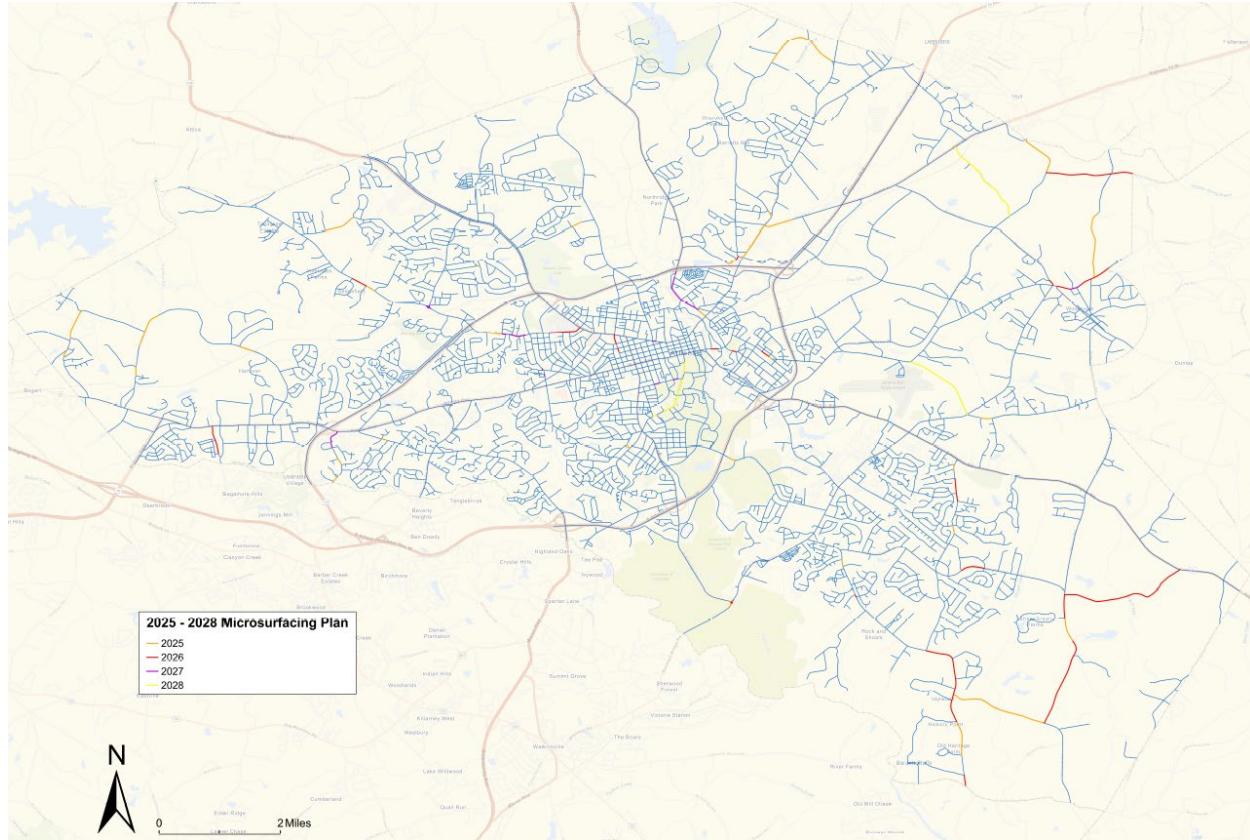


Figure 27. A 4-Year pavement preservation plan based on the 2024 budget.

Disclaimer: This pavement management report is intended to provide an overview of the 2023 condition and network-level management strategies based on the visual evaluation of surface distresses without including a comprehensive structural evaluation. Therefore, the precise pavement treatment should be determined based on the project-level analysis of the existing pavement conditions. The analyses presented herein are based on the information available during the assessment and may be subject to change over time. The County may elect to shift some segments from one year to another to group streets within a geographic area or to coincide with other capital projects.

6 Recommendations

- **Diversify Pavement Preservation Treatments** - The County might consider exploring and apply additional pavement preservation treatments which include microsurfacing, rejuvenator, etc. Pavement preservation treatments will keep good pavements in good conditions for longer period of time and will extend the service life reducing the maintenance costs.
- **Tailored Treatments for Low PCI Pavements** - For pavements with PCI less than 25 and base/subgrade issues, heavier treatments like FDR, should be applied to address structural concerns and ensure a strong foundation.
- **Data-Driven Decision Making** - GPR data from County-owned streets can inform treatment decisions accurately. This will allow the County to know the existing structure and plan for the required overlay thickness or FDR.

- **Structural Evaluation for Optimal Thickness** - Structural evaluation helps optimize pavement thickness for mill & inlay/reconstruction on arterial and collector streets.
- **Setting Target PCI's and Allowable Backlogs** - Set target PCI's based on functional class and maximum allowable backlogs and achieve them in 5-10 years.
- **Increased Funding for Sustainable Outcomes** - It is recommended to allocate additional funding to heavier treatments to improve the quality of roads and reduce backlogs, resulting in long-term cost savings.
- **Regular Pavement Condition Evaluations** - Evaluate pavement condition every four years to optimize management based on real-world results.

7 Conclusion

Pavements are the most valuable assets for any public agency. Investing in a pavement management program is rational; considering pavement management not only provides a consistent and rational management method to make decisions but also helps in the optimal use of funds and reduces pavement rehabilitation, resulting in extended pavement life and increased credibility with stakeholders. Based on the March 2023 survey, the County's average PCI value is 68.6, indicating the pavement network is in overall 'fair' condition. About 14.2% of the pavement areas (88 miles) are in 'marginal', 'poor', and 'very poor' conditions. It is recommended to consider increasing the annual fund in the future to offset the increased materials and construction costs and uncertainty in pavement performance, and additional damage incurred by the increased weight limit.

8 Appendix A

- A.1 A Map with 2023 PCI Values and Pavement Condition Categories
- A.2 List of Pavement Segments with 2023 PCI Values
- A.3 A Map with 2024 – 2028 Major Rehabilitation Plan
- A.4 List of Pavement Segments with 2024-2028 Major Rehabilitation Plan
- A.5 A Map with 2024-2028 Pavement Preservation Plan
- A.6 List of Pavement Segments with 2024-2028 Pavement Preservation Plan
- A.7 A Map with 2024 Localized Distress Maintenance Plan
- A.8 List of Pavement Segments with 2024 Localized Distress Maintenance Plan



PREPARED BY

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TSPL0ST 2018 - Monthly Project Update

For Period Ending January 01, 2025

01 – Airport Capital Improvements Program (CIP) Matching Funds

Project Manager:	Keith Sanders	Project Sponsor:	Airport
Project Status:	Active	Current Phase:	Re-Design
Original Budget:	\$ 1,531,000		
Budget Amendments:	\$ 538,642		
Grants:	\$12,793,021		
Total Project Funding:	\$14,862,663		
Expenses, Encumbered, Designated:	\$14,263,233	96%	
Available Balance:	\$ 599,431		

Project Details:

Sub-Project #3 – Taxiway "A" Reconstruction and Realignment

Construction Budget Amount: TBD

Status: Re-Design

Sub-Project #3 Details: The sub-project shall address the deteriorating pavement section for the majority of existing Taxiway "A" as well as several connecting taxiways by utilizing a combination of full depth reconstruction and asphalt overlay rehabilitation. In addition, the Terminal Apron will be expanded southward to provide additional aircraft parking. Related work will include grading, storm water, and taxiway safety area improvements. Taxiway edge lighting and signage, (new and relocated), will be incorporated in the project along with new pavement markings.

Sub-Project #3 Update: M&C approved the design services contract to develop the Project Concept on February 4, 2020. The Proposed Project Concept was deemed consistent with the initial project statement by TSPL0ST COC on March 16, 2020. On March 17, 2020, M&C approved the Project Concept and authorized staff to move the project onto the Bid and Award Phase for TSPL0ST 2018 Project #01 Sub-Project 3, Taxiway "A" construction. On April 2, 2020 ACCGov advertised for bids for the Taxiway "A" construction project. Bids were received on May 28, 2020. The award for the Schedule A portion of SP #3 was approved by M&C on July 7, 2020. Notice to Proceed was delayed by GDOT from original projection of March but it was issued on April 12, 2021. Unexpected shallow rock was encountered and a waterline could not be lowered, without considerable expense. A waiver has been received from the FAA and redesign work has been completed. Major Construction was completed October 02, 2024. Temporary markings have been applied. Permanent markings will be applied on November 03, 2024 after a 30 day cure period of the asphalt.

Sub-Project #4 – Taxiway "B" Pavement Rehabilitation and Lighting Project

Construction Budget Amount: \$

Sub-Project #4 Details: Taxiway "B" is the main taxiway to the secondary runway 2-20. The rehabilitation project will consist of milling the old pavement and replacing it with a soil cement base and then applying 4-6 inches of asphalt as a final layer. The project will include new lighting and signage along with new pavement markings. The current pavement is over 25 years old and is in poor condition. Poor condition is considered unsafe by FAA standards due to loose pavement that can be ingested by a jet engine or hit by an aircraft propeller.

Sub-Project #4 Update: The Project Concept for Taxiway "B" Pavement Rehabilitation and Lighting Project was approved by M&C on May 07, 2024. Design work has begun on the Taxiway B project. RS&H has completed 90% of the design and a meeting was held to go over the design for any changes or updates. Design anticipated completion by December 2024.

Anticipating GDOT funding for project in July 2025.

02 – Transit Vehicle Expansion & Replacement Project

Project Manager:	Keith Sanders	Project Sponsor:	Transit
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$ 4,000,000		
Total Budget Amendments:	(\$ 79,809)		
Grants:	\$ 4,059,930		
Total Project Funding:	\$ 7,980,121		
Expenses, Encumbered, Designated:	\$ 4,754,656	60%	
Available Balance:	\$ 3,225,464		

Project Details: Purchase up to 16 buses to replace existing vehicles that have reached the end of useful life. The funding may be used to purchase revenue vehicles and/or support vehicles and/or used to match funding for state and/or federal grants to acquire additional transit vehicles, as required.

Project Update: To date the Project has supplied Two 40' Hybrid heavy duty buses were put into service in March 2019, two more in February 2020, and two 35' Hybrid heavy duty buses in August 2020. All of these buses were paid with 90% matching grant administered by GDOT. Two additional buses were ordered January 2023. Delivery is anticipated November 2024.

03 – Bus Stop & Transfer Facilities Improvement Project

Project Manager:	Keith Sanders	Project Sponsor:	Transit
Project Status:	Active	Current Phase:	Land Acquisition
Original Budget:	\$ 500,000		
Budget Amendments:	\$ 37,339		
Grants:	\$ 585,874		
Total Project Funding:	\$ 1,123,213		
Expenses, Encumbered, Designated:	\$ 878,459	78%	
Available Balance:	\$ 244,754		

Project Details: Project 03 includes transit related improvements for the land acquisition, design, and construction of decentralized transit transfer facilities outside of the downtown terminal. Also, for the capital improvements of signage (static and/or electronic predictive arrival), concreate pads, seating, covered shelters over the paved pad, landscaping, lighting, route information, bicycle racks, trash cans, bus pull-offs, restroom facilities, bus/customer parking and other related bus stop improvements.

Project Update: ACCGov Transit has been awarded an FTA/GDOT 5339 discretionary grant for funding to identify and procure property, design, and complete a NEPA for a Remote Transfer Facility has been approved by GDOT and FTA. The approved grant is \$400,000, with a required \$100,000 local match, bringing the project total to \$500,000. This discretionary grant application that was approved by M&C and submitted in November 2019. It identified matching funds from the 2018 TSPLOST Project # 3, as local match. The original application was for \$1,000,000, with a \$250,000 local match. However, this grant only provides for \$400,000 of Federal funding, and requires a \$100,000 local match.

The Proposed Project Concept for the Transfer Facility for the Mall Redevelopment area was presented to the Oversight Committee on April 15, 2024 and the M&C approved the project concept on May 7, 2024. Schematic Design for the Mall Transfer Facility is on-going. Anticipate presentation to M&C early 2025.

Site Layouts and Traffic warrant analysis for a Transfer Facility at the SE Clarke Park with an associated Traffic Signal is underway. Anticipate Presentation to the M&C late 2024. The Proposed Final Site Selection and Proposed Schematic Design was approved by at the regular session meeting on December 3, 2024.

05 – Oconee Rivers Greenway Project

Project Manager:	Derek Doster	Project Sponsor:	Leisure Services
Project Status:	Active	Current Phase:	Varies by Sub-Project
Original Budget:	\$ 10,000,000		
Budget Amendments:	\$ 361,345		
Grants:	\$ 0		
Total Project Funding:	\$ 10,361,345		
Expenses, Encumbered, Designated:	\$ 9,945,044	96%	
Available Balance:	\$ 416,301		

Sub-Project #7 – Oak/Oconee Bridge Underpass

Construction Budget Amount: \$1,000,000

Status: Permitting

Sub-Project Details: Sub-Project #7 is to provide an at grade connection between the greenway trail sections on either side of the Oak/Oconee Street bridge. The project is being planned to be built at the same time as the GDOT Bridge over Oak/Oconee Street is being replaced.

Sub-Project Update: Coordination with GDOT on design, permitting and schedule is on-going. ACCGov has contracted with the firm Gresham Smith to perform the design and permitting portions of the greenway trail under the bridge at the same time as they are doing the work for GDOT. The Project was presented at M&C Work Session on May 12, 2020 which can be viewed at https://youtu.be/6y4GXn0A_2s?t=12822. On June 2, 2020 M&C approved the preliminary plans. GDOT continues to work on the Final Design, heading to a December 2023 date. Geotechnical borings were completed on September 8, 2021. Easements from ACCGov to GDOT have been completed.

GDOT let their project for construction in June 2024 and it was awarded to ER Snell, Inc. with a 24-month construction period. Staff is finalizing permitting with GDOT and ACCGov departments with anticipated bidding effort in the winter 2025.

Sub-Projects #9: Sub-Project #9 is to provide an at grade connection along the river and MLK between North Avenue and East Broad Street connecting to existing greenway trails.

Sub-Project Update: Sub-Project #9 was approved by M&C for staff to submit GDOT grant application for construction funding. GDOT awarded a planning grant with GDOT-ACCGov kick off meeting schedule for April 2023.

See TSPLOST 2023 Project 11, East Athens MLK Parkway Oconee River Greenway Trail Project, for any further updates for SP #9.

Art installation for the North Oconee Bridge Mural is complete.

06 – Firefly Trail Project

Project Manager:	Derek Doster	Project Sponsor:	Leisure Services
Project Status:	Active	Current Phase:	Land Acquisition
Original Budget:	\$ 16,752,000		
Budget Amendments:	\$ 1,914,455		
Donations:	\$ 51,395		
Total Project Funding:	\$ 18,717,850		
Expenses, Encumbered, Designated:	\$ 18,484,747	99%	
Available Balance:	\$ 233,103		

Sub-Project #3 – Section from Spring Valley Road to ACC Line

Construction Budget: To be funded from TSPLOST 2023

Current Phase: Land Acquisition & Construction

Sub-Project #3 Details: This Sub-Project includes the necessary land acquisition efforts, as funding allows, from Hancock Road/Spring Valley Road intersection and ends at Moores Grove Road/Athens Road intersection. The Project Concept for Sub-Project #3 was approved by M&C on July 5, 2016. Approval to acquire the parcels near and adjacent to Ranick Road and Spring Valley Road is was given by M&C on November 6, 2018. The Preliminary Plans for the first portion of Sub-Project #3 were approved by M&C on October 6, 2020. Final Design on the first section of Sub-Project #3 is proceeding according to schedule. The Preliminary Plans for the remainder of Sub-Project #3, using the original rail bed or Alternate "A" alignment was approved by M&C on March 3, 2021.

Sub-Project #3 Update: Land Acquisition efforts continue, including title opinion reports and appraisals. Construction bidding has been completed. Construction Notice to Proceed could follow within 1-3 months and be complete in 12-14 months. On September 3, 2024 at the regular session meeting M&C approved the construction services award.

The Art installations for both the intersection and the trailhead are underway. The Artists are in the fabrication phase. [The art at the trailhead is expected to be installed late January 2025.](#)

08 – Tallassee Road Bridge Replacement Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Varies by Sub-Project
Original Budget:	\$ 6,000,000		
Budget Amendments:	\$ 1,088,678		
Other Funds:	\$ 490,554		
Grant Funds:	\$ 750,000		
Total Project Funding:	\$ 8,415,210		
Expenses, Encumbered, Designated:	\$ 7,878,054	95%	
Available Balance:	\$ 427,308		

Sub-Project #1 Project Details: The project is complete.

Sub-Project #2 Details: Public Art to be associated with the Bridge Project.

Sub-Project #2 Update: Public Art location, center of the adjacent round-about, was approved by M&C February 2023. Athens Cultural Affairs Commission has not yet issued a call for art.

09 – Downtown Transportation Improvements Project

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Close-out
Original Budget:	\$ 7,097,000		
Budget Amendments:	(\$ 741,312)		
Grants:	\$ 0		
Total Project Funding:	\$ 6,355,688		
Expenses, Encumbered, Designated:	\$ 3,778,687	59%	
Available Balance:	\$ 2,577,001		

Sub-Project #1 Project Details: The project is complete.

11 – Bicycle Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Design
Original Budget:	\$ 6,000,000		
Budget Amendments:	\$ 739,466	(Amendments: Prog. Admin. separated, & Debt Service reduced)	
Grants:	\$ 0		

Total Project Funding:	\$ 6,739,466	
Budgeted for SP #1	\$ 1,445,000	
Budgeted for SP #2	\$ 0	
Budgeted for SP #3	\$ 1,288,501	
Expenses, Encumbered, Designated:	\$ 6,098,183	90%
Available Balance:	\$ 641,283	

Project Details: Project 11 includes capital improvements on routes for the bicycle projects as may be contained in the 2017 Bicycle / Pedestrian Master Plan once approved by Mayor and Commission, and as may be amended from time to time. Bicycle Master Plan may include additional roadway bicycle lanes, roadway pavement marking, off-road bicycle paths, trails, related signage, and/or bicycle related amenities.

Sub-Project #1 - Cherokee Road multi-use path: See Project 12 Sub-Project #1 below for details

Sub-Project #2 – Riverbend Road multi-use path: See Project 12 Sub-Project #2 below for details

Sub-Project #3 – Jefferson River Road multi-use path: See Project 12 Sub-Project #3 below for details

Sub-Project #7 – Barber Street Multi-Use Path and Sidewalks: See Project 12 Sub-Project #7 below for details

12 – Pedestrian Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Design
Original Budget:	\$ 11,000,000		
Budget Amendments:	\$ 156,303		
Grants:	\$ 0		
Total Project Funding:	\$ 11,156,303		
Expenses, Encumbered, Designated:	\$ 8,416,131	75%	
Available Balance:	\$ 2,740,172		

Project Details: Project 12 includes pedestrian improvements as contained in the 2017 Bicycle / Pedestrian Master Plan, as approved by Mayor and Commission, and as may be amended from time to time. The project will provide for land acquisition, design, and construction sidewalk improvements and/or installation of pedestrian safety devices such as stamped and colorized crosswalks, flashing crosswalks, ADA compliant elements, pedestrian signage, pedestrian countdown timers, and other safety equipment.

Sub-Project #1 - Cherokee Road multi-use path - Between Lexington and Beaverdam Road

Construction Budget: N/A

Designer: Arcadis U.S., Inc.

Status: Preliminary Design

Sub-Project #1 Details: Cherokee Road- Connect a sidewalk gap on Cherokee Road between Lexington Road to Beaverdam Road on the southeast side of the road to avoid an existing cemetery. Pedestrian access to Satterfield Park will also be considered. Connection points to the existing sidewalk are at Morning View Drive and 360 feet down Beaverdam Road Project length is approximately 3,700 feet.

Sub-Project #1 Update: Concept design was presented to the public for comment on October 14, 2019 and was presented to M&C on November 12, 2019. M&C directed concepts be developed further before being brought to M&C for consideration. Staff presented multi-use path and sidewalk option in the July 9, 2020 M&C Work Session. At the September 1, 2020 vote meeting M&C approved by Commission Defined Option the Project Concept for the Cherokee Road Sub-Project and directed staff to advance the project to the Preliminary Plans phase as a Multi-Use Path. On October 28, 2020 a NOPAA was approved by the Mayor & Commission to use the on-call design engineers, Arcadis US, Inc. to continue advancing the Sub-Project through preliminary engineering and design.

M&C approved the Preliminary Plans on November 6, 2024 and authorized staff to enter the land acquisition and final design phase to support bidding the project. Targeted completion dates for land acquisition and final design are August 2025.. Construction funds will be supported by TSPLSOT 2023 Project # 25 Bicycle and Pedestrian Improvements.

Sub-Project #2 – Riverbend Road Between Milledge & College Station Road.

Construction Budget: \$2,900,000 with additional funding from TSPLSOT 2023

Designer: Arcadis U.S., Inc.

Status: Concept Design

Sub-Project #2 Details: The sub-project is to Project Concept Design for sidewalk along one side of Riverbend Road connecting the sidewalk College Station Road to S. Milledge Ave.

Sub-Project #2 Update: Concept design was presented to the public for comment on October 14, 2019 and was presented to M&C on November 12, 2019. M&C directed concepts be developed further before being brought to M&C for consideration. Project Concept was approved by M&C on March 2, 2021. Preliminary Plans have been submitted to Transportation & Public Works staff for review. Refinement of the design near the intersection of College Station Road, final storm water systems, and updates per review comments are being incorporated. Final bidding date has not been established.

Sub-Project #3 – Jefferson River Road multi-use path

Construction Budget: \$2,479,700

Designer: TyLIN, Inc.

Status: Concept Design

Sub-Project Details: This sub-project is to provide a Project Concept Design for sidewalk along one side of Jefferson River Road connecting the sidewalk on the south side of Jefferson Road to Vincent Drive.

Sub-Project Update: Concept design was presented to the public for comment on October 14, 2019 and was presented to M&C on November 12, 2019. M&C directed concepts be developed further before being brought to M&C for consideration. Staff presented the multi-use path option at M&C Work Session on September 8, 2020. On November 3, 2020 M&C approved the Jefferson River Road multi-use path Project Concept. Preliminary Plans are being developed.

Sub-Project #7 – Barber Street Multi-Use Path and Sidewalks

Construction Budget: \$2,479,700

Designer: Benesch/Toole

Status: Concept Design

Sub-Project Details: This sub-project is to provide Bicycle and Pedestrian Improvements along Barber Street from Dairy Pak Road to Prince Avenue. The Project Concept for fills a connectivity gap on Barber Street which provides improved bicycle and pedestrian access and safe corridor connectivity from Newton Bridge Road to Prince Ave, including commercial businesses, bus stops, and a number of apartment complexes and residential properties in the area. The project consists of: Approximately 7,230 feet of two-way separated bike facility and 5-foot sidewalk starting from N. Chase Street (Newton Bridge Road to Prince Ave) to Boulevard (Segments 1, 2, 3 and 4); Intersection improvements at N. Chase Street, Oneta Street, Boulevard, and multiple commercial driveways and side streets, including ADA improvements with accessibility ramps, pushbutton stations, and crosswalks; and Minor intersection improvements on Prince Ave.

The overall Barber Street Bike and Pedestrian Improvements Project is broken up into four different segments:

1. From North Chase Street to the Loop 10 underpass
2. From Loop 10 underpass to Oneta Street
3. From Oneta Street to Boulevard
4. From Boulevard to Prince Avenue

The majority of funding for Design is coming from SPLOST 2011 Project 06, Sub-Project #6 but the majority of construction funding is proposed to come from TSPLSOT 2018 Project 11 and 12, Sub-Project #7.

Sub-Project Update: Project concept was presented to the TSPLSOT Oversight Committee May 24, 2021. Project Concept presented to M&C in July 2021. AiM approved a recommendation for the Proposed Project Concept on July 13, 2021. M&C requested additional design options for the sub-project before approving the Project Concept. Revised Project Concept was presented to M&C at the January 12, 2023 Work Session. On February 7, 2023 M&C approved a motion tabling the decision on the proposed revised project concept.

On June 6, 2023, the M&C unanimously approved a Commission Defined Option for this Sub-Project's Project Concept. The CDO and approved Project Concept can be found at the following link [Approved Project Concept](#). (The linked CDO and agenda report contains links to all of the previous M&C Work Session Videos and agenda reports.)

The M&C June 6, 2023, discussion regarding the project and the approved CDO can be found at

<https://youtu.be/TqExKGdHnXY?list=PLeQe7iIDuV5IVL88jXIJvEr6EQ0kzw6QF&t=9260>.

Site walk with M&C was held on December 15, 2023. Survey is complete. A second site walk was held on July 18, 2024 with initial layouts based on survey data. Staff is meeting with Segment 4 property owners to discuss property impacts per the CDO. Preliminary Plans are ongoing with proposed presentation to M&C in Winter 2025.

13 – W. Broad Area Pedestrian Improvements Project

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Design
Original Referendum Budget:	\$ 4,000,000		
Budget Amendments:	(\$ 139,809)		
Grants:	\$ 0		
Total Project Funding:	\$ 3,860,191		
Expenses, Encumbered, Designated:	\$ 1,590,172	41%	
Available Balance:	\$ 2,270,019		

Project Details: Potential project options include sidewalks, multi-use trail, pedestrian traffic lights, traffic management devices and other general streetscape improvements to improve pedestrian movement within the W. Broad neighborhood area.

1. M&C Approved Project Concept	December 4, 2019 – Complete
2. Georgia Department of Transportation (GDOT) approved the Concept Report	August 5, 2021 – Complete
3. GDOT held their Avoidance & Minimization Measure Meeting	September 13, 2021 – Complete
4. TPW staff presented at M&C Work Session on	November 9, 2021
5. GDOT hosted a virtual Public Information Open Houses	May 2022 – Complete
6. M&C Approved Preliminary Plans and Right-of-Way Plans	April 4, 2023 – Complete
7. Final Plans	Late 2024 - Complete
8. Right-of-Way Acquisition	Summer/Fall 2024
9. Construction Start	Summer/Fall 2025

Project Update: Final Design is underway with right-of-way (ROW) Acquisition and Construction Advertisement Phases per the GDOT PDP Process. Discussions have begun with impacted property owners. GDOT has agreed to the use of raised crosswalks on The Plaza, Minor Street and West Hancock Avenue. Utility locations are being confirmed. Received environmental certification and location & design approval in late February 2024. GDOT approved ROW plans in mid-March 2024. The long-range schedule includes Final Plans in late 2024. ROW acquisition is underway, and Construction Let in July 2025.

14 – Lexington Highway Corridor Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Design & ROW
Original Budget:	\$ 4,000,000		
Budget Amendments:	(\$ 139,809)		
Grants:	\$ 0		
Total Project Funding:	\$ 3,860,191		
Expenses, Encumbered, Designated:	\$ 869,012	23%	
Available Balance:	\$ 2,991,179		

Project Details: The Lexington Highway Corridor Improvement Project is seeking to provide infrastructure improvements to help improve traffic flow, pedestrian access, and bicycle facilities between Johnson Road (east of the Athens Loop) and Southeast Clarke Park. Project information and studies can be found at the following location: <https://www.acgov.com/lexingtonhighway>.

1. M&C Approved a list of prioritizations for future Project Concepts	July 7, 2020 – Complete
2. User Group developed Project Concepts	July 2020 to January 2021 – Complete

3. Proposed Project Concepts Presented to M&C	January 12, 2021 – Complete
4. Public Input on Proposed Project Concepts	January 12, 2021 – April 18, 2012 - Complete
5. M&C Approved Project Concepts & Authorized Preliminary Plans	September 7, 2021 – Complete
6. M&C Approved Preliminary Plans and Right-of-Way Plans	August 2, 202 – Complete
7. Right of Way Acquisition	June 2023 – late 2024
8. Final Plans	Late 2024
9. Construction Start	Early 2025

Project Update: When the prioritization list was approved, the implementation of the “Immediate Impact Project” of airport wayfinding signage was also approved.

Construction plans have been reviewed by GDOT with right-of-way documents phase authorized.

Final Construction Plan revisions have been completed and land acquisitions are underway. Right of way/Easement acquisition completion anticipated December 2024

15 – Atlanta Highway Corridor Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Preliminary Design
Original Budget:	\$ 4,000,000		
Budget Amendments:	(\$ 139,809)		
Grants:	\$ 0		
Total Project Funding:	\$ 3,860,191		
Expenses, Encumbered, Designated:	\$ 3,150,923	82%	
Available Balance:	\$ 709,268		

Project Details: Project information and studies can be found at the following location: <https://www.acgov.com/atlantahighway>. From May 28, 2020 to June 28, 2020, ACCGov received online public input. On July 7, 2020, M&C approved the proposed project list, authorized the Project list for prioritization into the Project Concept Phase, and authorized the implementation of the “Immediate Impact Projects”.

From July 2020 to present, the User Group and Design Professionals have been developing Project Concept materials for each of the proposed projects; which was presented to the M&C at the January 12, 2021 work session. The recent Public Engagement opportunities were completed on April 18, 2021. The User Group has begun reviewing the public input results to help develop a formal priority project list for recommendation to Mayor and Commission. The priority recommendation is scheduled to be presented to M&C during the August work session. The Public Engagement efforts yielded 642 responses from 203 unique commenters.

Immediate impact projects, such as additional signage in front of Georgia Square Mall to better notify drivers that the mall turn lane ends ahead, have been installed. ACCGov Landscape Management has received the Georgia Department of Transportation (GDOT) permit to install landscaping on Atlanta Highway during the next planting season.

On September 7, 2021, M&C approved a Commission Defined Option (CDO) detailing an expansion of work recommended by the TSPLOST 2018 – Atlanta Corridor Improvements Project User Group. On December 6, 2022, M&C approved another CDO the approving Proposed Project Concepts for TSPLOST 2018 – Atlanta Corridor Improvements Project and authorized the project to enter into the Preliminary Plan Phase. Presentation of the Proposed Preliminary Plans were approved by M&C in November 2023 with the direction that TSPLOST 2023 would be the primary funding source for the construction of these two Sub-Projects.

Project Update:

Sub-projects for the shared-use path for Timothy Road and Mitchell Bridge Road have been transitioned to the TSPLOST 2023 Program Project #6, as two separate sub-projects as approved by M&C.

16 – Prince Avenue Corridor Improvements Project

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	In-Active	Current Phase:	Planning

Original Budget:	\$ 4,000,000	
Budget Amendments:	(\$ 139,809)	(Amendments: Prog. Admin. separated, & Debt Service reduced)
Grants:	\$ 0	
Total Project Funding:	\$ 3,860,191	
Expenses, Encumbered, Designated:	\$ 1,364,488	35%
Available Balance:	\$ 2,495,703	

Project Details: Project 16 includes funding for specific improvements to the Prince Avenue Corridor that are to be approved by Mayor and Commission. Improvements eligible for the funding include, but are not limited to, landscaped/concrete median(s), additional sidewalks, multi-use trail, separated bike lanes, and intersection improvements at the intersections of N. Milledge Avenue, King Avenue, and Park Avenue/Talmadge Drive. The User Group held its kickoff meeting on November 9, 2020. Regularly scheduled meetings are held on the fourth Wednesday of each month at 3:00 pm. Public engagement concluded in September 2021. The User Group presented a recommended project priority to M&C at the January 11, 2022 Work Session. The Project Priority List was approved by the M&C at the February 1, 2022 Regular Session.

Sub-Project #1 – Permanent Road Diet as Amended

Sub-Project Update: At the December 13, 2022 Meeting, M&C approved advancing the Project Concept into Preliminary Plan Phase for long-term peripheral improvements including addressing curb cuts, different buffer materials, ADA and crosswalk improvements, and intersection improvements. M&C approved a [Commission Defined Option \(CDO\)](#) for the proposed Preliminary Plans at the December 3, 2024 Regular Session. [The Design Engineer is addressing comments from the approved CDO.](#)

Sub-Project #3 - Prince Avenue Load Zone on Meigs Street

Sub-Project Update: At the February 6, 2024, Regular Meeting, M&C approved a CDO for Sub-Project #3 (Prince Avenue Load Zone on Meigs Street). Phase I of the CDO is complete. Phase II will occur after the Bottleworks parking lot development is complete.

Sub-Project #4 - Mid-Block Crossings near Social Security Office and in Normal town

Sub-Project Update: The Design Engineer is working on the Project Concept for Sub-Project #4 (Mid-Block Crossings near Social Security Office and in Normal Town). These crossings are on hold per GDOT request.

Sub-Project #5 - Pilot Green Infrastructure at the Cobb Street intersection

Sub-Project Update: M&C approved a [CDO](#) for the proposed Preliminary Plans at the December 3, 2024 Regular Session.

17 – Acquisition of Private Streets Project

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	In-Active	Current Phase:	Waiting future submissions
Original Budget:	\$ 340,000		
Budget Amendments:	(\$ 11,806)		
Grants:	\$ 0		
Total Project Funding:	\$ 328,194		
Expenses, Encumbered, Designated:	\$ 77,909	24%	
Available Balance:	\$ 250,285		

TSPLIST 2018 Completed Projects and Sub-Projects

01 – Airport Capital Improvements

- **Sub-Project #1 – Runway 9-27 Rehabilitation – completed March 2019**
- **Sub-Project #2 – Localizer Replacement**

03 – Bus Stop & Transfer Facilities Improvement Project

- **Bus Stop Improvements – Completed July 2021**

04 – Transit Service Expansion Projects

- Sub-Project #1 – Route Expansion Project – completed July 2020

05 – Oconee Rivers Greenway Project

- Sub-Project #2 – Oconee Hills Cemetery Section – October 2022
- Sub-Project #4 & #6 - Carr's Creek / Barnett Shoals Road
- Sub-Project #8 – MLK Greenway Extension – completed October 2019
- Sub-Project #8 – Public Art – completed August 2021
- Sub-Project – Public Art – completed October 2024

06 – Firefly Trail Project

- Sub-Project #2 – Old Winterville Road to Hancock Road – completed October 2021
- Sub-Project #3.1 – Winterville Section – completed October 2020

07 – Pavement Rehabilitation Project

- Sub-Project #1 - CY2018/FY19 PMP – Completed spring 2020
- Sub-Project #2 - CY2019 PMP – Completed spring 2020
- Sub-Project #3 - CY2020 PMP – Completed spring 2021
- Sub-Project #4 - CY2021 PMP – Completed spring 2022
- Sub-Project #5 - CY2022 PMP – Completed spring 2023
- Sub-Project #6 - CY2023 PMP – Completed spring 2024

08 – Tallasseee Road Bridge Replacement Project

- Sub-Project #1 – Final Completion June 2024

09 – Downtown Transportation Improvement Project

- Sub-Project #1 – Final Completion November 2022
- Sub-Project #2 – Public Art – completed December 2022

10 – Whitehall Road at S. Milledge Avenue Project – completed September 2019

- Sub-Project – Public Art – completed September 2021

12 – Pedestrian Improvements Program

- West Broad MMSAG Sidewalk Project
- King Avenue Sidewalks
- FY17 Sidewalk Gap

16 – Prince Avenue Corridor Improvements

- Sub-Project #1 – Road diet with separated bike lanes from Milledge Avenue to Pulaski Street
- Sub-Project #2 – Pedestrian signals and crosswalks at Park Avenue/Talmadge Drive and Prince Avenue
- Sub-Project #3 – Meigs Load Zone – Phase I CDO April 2024

17 – Acquisition of Private Streets Project

- Sub-Project #1 – Laurel Springs Subdivision

18 – City of Winterville**19 – Town of Bogart**

TSPLIST 2023 Monthly Project Update

For Period Ending 1/01/2025

01 - Winterville Storm Drainage Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Preliminary Design
Original Budget:	\$2,712,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Total Project Funding:	\$2,712,000		
Expenses, Encumbered, Designated:	\$ 427,217	16%	
Available Balance:	\$2,284,783		

Project Details: This project will provide for capital improvements for the repair, upgrade, and/or replacement of failed or failing stormwater pipes within the transportation related stormwater systems on or in the vicinity of Main Street in Winterville.

Project Update: Project Concept was approved by M&C February 6, 2024. Survey work has been completed. An affected property owner's meeting was held July 30, 2024. [The project Preliminary Design, was presented in the August 2024 Work Session.](#) On September 3, 2024 M&C approved the Preliminary Design Plans and approval to begin land acquisition. [The Final Design Plans were submitted to ACCGov in December 2024 for review and comments.](#)

Expected Schedule (confirmed by consultant)

- Final Design Completion January 2025
- Land Acquisition Completion June 2025
- Start Construction Bid & Award August 2025
- Construction NTP November 2025

02 - Lexington Road Corridor Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Final Design
Original Budget:	\$8,190,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Total Project Funding:	\$8,190,000		
Expenses, Encumbered, Designated:	\$ 805,660	10%	
Available Balance:	\$7,384,340		

Project Details: This project will provide for capital improvements to the Lexington Road Corridor.

Project Update: The User Group met on March 14, 2024 to determine priority sub-projects. The Sub-project recommendation presentation to AIM was on March 26, 2024. The proposed sub-project concepts were presented during the M&C Work Session on August 13, 2024. On September 3, 2024 at the regular session meeting, M&C approved the Proposed Sub-Project Concepts. The proposed Design Service Contract Award is scheduled for M&C approval in the Dec/Jan cycle.

03 - Lexington Road Sidewalk Gap Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$532,000		
Budget Amendments:	\$ 0		
Total Project Funding:	\$532,000		
Expenses, Encumbered, Designated:	\$ 77,376 15%		
Available Balance:	\$454,624		

Project Details: This project will provide funding for capital improvements on the north side of Lexington Road between Transit Stop near the with Cooper Road and the sidewalk in front of Wal-Mart at Whit Davis Road.

Project Update: The Project Concept Design is underway. Affected property owners were contacted with plans for review. Project Concept Design was presented in tandem with Lexington Road Corridor during the M&C Work Session on August 13, 2024. a. On September 3, 2024 at the regular session meeting M&C approved the proposed Project Concept. On December 3, 2024 at the regular season meeting M&C approved the proposed Preliminary Design

04 - Atlanta Highway and W. Broad Street Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$8,722,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Total Project Funding:	\$8,722,000		
Expenses, Encumbered, Designated:	\$ 406,000 5%		
Available Balance:	\$8,316,000		

Project Details: This project will provide for capital improvements to the Atlanta Highway / W. Broad Street Corridor.

Project Update: The User Group for this project has not yet been selected by Manager's Office.

05 - Prince Avenue/Jefferson Road Corridor Improvements Project

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$8,722,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Total Project Funding:	\$8,722,000		
Expenses, Encumbered, Designated:	\$ 500,560 6%		
Available Balance:	\$8,221,440		

Project Details: This project will provide for capital improvements to the Prince Avenue / Jefferson Road Corridor.

Project Update: The User Group kick-off meeting was held on January 24, 2024. The Design Engineers completed traffic counts and presented intersection improvement schematics and conceptual cost estimates at the September 25th User Group Meeting. Potential bicycle and pedestrian projects and conceptual cost estimates were presented at the October 23rd User Group meeting. The User Group has begun working on developing a Proposed Project Concept.

06 - Timothy Rd/Mitchell Bridge Rd Corridor Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$6,762,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Total Project Funding:	\$6,762,000		
Expenses, Encumbered, Designated:	\$ 375,127 6%		
Available Balance:	\$6,386,873		

Project Details: This project will provide capital improvements to the Timothy Road and Mitchel Bridge Road Corridor Improvements. This project will provide the construction funding for the improvements that are currently being designed as part of the TSPLOST 2018 – Atlanta Corridor Improvements Project.

Project Update: On September 7, 2021, M&C approved a Commission Defined Option (CDO) detailing an expansion of work recommended by the TSPLOST 2018 – Atlanta Corridor Improvements Project User Group. On December 6, 2022, M&C approved another CDO the approving Proposed Project Concepts for TSPLOST 2018 – Atlanta Corridor Improvements Project and authorized the project to enter into the Preliminary Plan Phase. Presentation of the Proposed Preliminary Plans were approved by M&C in November 2023 with the direction that TSPLOST 2023 would be the primary funding source for the construction of these two Sub-Projects.

Timothy Road final plans are underway. The designer is currently developing property impacts/easement drawings. The GDOT Traffic Operations coordination called for signal upgrades for pedestrian crossings was completed.

07 - E. Athens Neighborhood Pedestrian and Lighting Safety Project

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$37,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$4,000 3%		
Available Balance:	\$ 33,000		

Project Details: This project will provide for capital improvements to provide for improvements to street and pedestrian lighting for improved safety and accessibility in the E. Athens Neighborhood Area that are to be approved by the Mayor and Commission.

Project Update: The User Group has been meeting and reviewing all of the available data for the different types of improvements proposed for East Athens Neighborhood. The User Group is working to develop Proposed Project Concepts for Public Engagement and consideration by the M&C.

08 - E. Athens Neighborhood Traffic Management Program

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$141,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Total Spent or Encumbered:	\$ 13,000 9%		
Available Balance:	\$128,000		

Project Details: This project will provide for capital improvements to address speeding motorists, excessive traffic volumes, and/or related safety concerns on local residential streets through the Neighborhood Traffic Management Program Policy, as may be amended by the Mayor and Commission from time to time.

Project Update: See Update for Project 07.

09 - E. Athens Neighborhood Pavement Rehabilitation Project

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$239,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated :	\$ 20,134 8%		
Available Balance:	\$218,866		

Project Details: This project provides funding for ongoing necessary preventive maintenance, road reconstruction, and rehabilitation activities for various streets and roadways in the E. Athens Neighborhood Area.

Project Update: See Update for Project 07.

10 - E. Athens Neighborhood Transit Improvements Project

Project Manager:	Keith Sanders	Project Sponsor:	Transit
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$354,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		

Expenses, Encumbered,
Designated: \$ 32,000 9%
Available Balance: \$322,000

Project Details: This project will provide capital funding for additional transit improvements in the E. Athens Neighborhood Area.

Project Update: See Update for Project 07.

11 - E. Athens MLK Parkway Oconee River Greenway Trail Project

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$2,408,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 223,763 9%		
Available Balance:	\$2,184,237		

Project Details: This project will provide for capital improvements for the continuation of the design and construction an at grade connection along the river and MLK between North Avenue and East Broad Street connecting to existing greenway trails. The Project Concept for this work was approved by M&C March 6, 2018 as part of the TSPLOST 2018 Project 05, Oconee Rivers Greenway Project, Sub-Project #9. There was not in adequate funding for the design and construction in the TSPLOST 2018 Program but is now stand alone project for TSPLOST 2023. M&C approved a GDOT grant application for construction funding. However, GDOT awarded a planning grant with GDOT-ACCGov. A general kick-off meeting was held in April 2023 with GDOT executing a Project Framework Agreement (PFA) on March 12, 2024 outlining GDOT will provide \$400,000 in planning and design support. Also, the project will coordinate improvement opportunities at the North Avenue/MLK Drive intersection that is in planning phase as part of the larger North Avenue Raise Grant Project to improve the North Avenue corridor project being funded by a \$25 Million grant from the Federal Highway Administration Project through Senator Warnock's office.

Project Update: ACCGov received responses to the Request for Qualifications (RFQ) from Design Firms on May 9, 2024. ACCGov is in the process of the evaluation of the RFQ submissions to identify the best apparent firm for design services. All processes must follow GDOT procedures as part of the PFA to utilize the grant funding. M&C voted to approve the Design Services Contract on October 01, 2024.

12 - E. Athens Neighborhood Greenway Connectors Project

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$1,496,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 208,000 14%		
Available Balance:	\$1,288,000		

Project Details: This project will provide for capital improvements for the continuation of the Greenway Trail connector(s) in the E. Athens Neighborhood Area.

Project Update: The User Group kick-off meeting was held on April 29, 2024; established a schedule of two monthly meetings.

13 - E. Athens Neighborhood Pedestrian Improvements Project

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$971,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 92,000 9%		
Available Balance:	\$879,000		

Project Details: This project will provide funding for capital improvements to pedestrian accessibility and safety in the E. Athens Neighborhood Area.

Project Update: See Update for Project 07.

14 - N. Athens Transportation Improvements Program

Project Manager:	Keith Sanders	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$8,041,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 332,000 4%		
Available Balance:	\$7,709,000		

Project Details: This program will provide capital funding for transportation improvements in the north Athens area.

Project Update: The User Group kick-off meeting was held on October 08, 2024

15 - Sycamore Drive Pedestrian Improvements Project

Project Manager:	Mike Kajder	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$1,470,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 170,330 12%		
Available Balance:	\$1,299,670		

Project Details: This project will provide for capital improvements for pedestrian improvements in the general vicinity of Sycamore Drive to provide improved pedestrian connectivity to Atlanta Highway, surrounding transit stops, businesses, and/or residential areas.

Project Update: Kick-off meeting with T&PW was held on May 8, 2024. The proposed Project Concept [Work Session was held on November 17 2024](#). The Proposed Project Concept was scheduled to be presented to the TSPLOST Oversite Committee on November 15, 2024, but there was no quorum. The Proposed Project Concept was scheduled to be presented to the TSPLOST Oversite Committee in December, 2024, but there was no quorum present. The Proposed Project Concept was approved by Mayor and Commission on January 7, 2025.

16 - Westchester Neighborhood Area Pedestrian Improvements Project

Project Manager:	Mike Kajder	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$1,892,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$192,120 10%		
Available Balance:	\$1,699,880		

Project Details: This project will provide for capital improvements for improving safety, accessibility, and connectivity of Westchester neighborhood areas to transit, Mitchell Bridge Road and/or Tallassee Road.

Project Update: The Kick-off meeting with T&PW was held on May 8, 2024. Project Designer selection has occurred and design contract execution is underway.

Expected Schedule:

- M&C Project Concept Work Session +12 weeks
- Preliminary Plan Approval +26 weeks
- Land Acquisition Approval +26 weeks
- Final Design Completion +12 weeks
- Land Acquisition Completion TBD
- Start Construction Bid & Award +12 weeks
- Construction NTP +12 weeks

17 - Stonehenge Neighborhood Area Pedestrian Improvements Project

Project Manager:	Daniel Garren	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$4,612,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 197,000 4%		
Available Balance:	\$4,415,000		

Project Details: This project will provide for capital improvements for improving safety, accessibility, and connectivity along Old Monroe Road and accompanying streets in the Stonehenge neighborhood including, but not limited to, Stonehenge Way, Marlborough Downs Road, Heelstone Ave, and/or Altarstone Drive.

Project Update: Staff has finalized POND as the design firm for the project and will hold a kick-off meeting on January 8. Staff is working with GIO to provide the design firm the necessary GIS information for the neighborhood to develop Project Concept drawings. Staff anticipates the Project Concept to go to M&C for approval at the April/May voting session.

18 - Firefly Trail Project

Project Manager:	Derek Doster	Project sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$7,345,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		

Expenses, Encumbered,
Designated: \$ 341,844 5%

Available Balance: \$7,003,156

Project Details: This project will provide for capital improvements for the final section of the Firefly Trail known as Sub-Project #3 (SP #3) Section from Spring Valley Road to current end of the Firefly Trail at the Winterville City Limits. The Project Concept for SP #3 was approved by M&C on July 5, 2016. Approval to acquire the parcels near and adjacent to Ranick Road and Spring Valley Road was given by M&C on November 6, 2018. The Preliminary Plans for the first portion of SP #3 were approved by M&C on October 6, 2020. The Preliminary Plans for the remainder of SP #3, using the original rail bed or Alternate "A" alignment was approved by M&C on March 3, 2021. Final Design has been completed.

Project Update: Land Acquisition efforts continue, including title opinion reports and appraisals. Construction bidding has been completed. . Construction Notice to Proceed could follow within 1-3 months and be complete in 12-14 months. On September 3, 2024 at the regular session meeting M&C approved the construction services award.

21 - Pavement and Bridge Maintenance Program

Project Manager:	Transportation and Public Works	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$21,751,000		
Budget Amendments:	\$ 2,586,171		
Grants:	\$ 3,620,337		
Expenses, Encumbered, Designated:	\$17,415,203 62%		
Available Balance:	\$10,542,306		

Project Details: This program will provide continuation of preventive road and bridge maintenance, road reconstruction, and/or rehabilitation activities for various streets and roadways throughout ACC.

Project Update: This project is being managed by ACCGov Transportation & Public Works Department.

22 - Culvert and Live Stream Pipe Replacement Program

Project Manager:	Transportation and Public Works	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$3,487,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered Designated:	\$1,198,482 34%		
Available Balance:	\$2,288,518		

Project Details: This program will provide funding for the repair, upgrades, and/or replacement of failed or failing stormwater culverts and/or live stream pipes within the transportation-related stormwater systems of ACC.

Project Update: This project is being managed by ACCGov Transportation & Public Works Department.

23 - Greenway Trail Safety Improvements Project

Project Manager:	Derek Doster	Project Sponsor:	Leisure Services
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$1,693,000		
Budget Amendments:	\$ 750,000		

Grants:	\$ 0
Expenses, Encumbered, Designated:	\$1,917,9631 79%
Available Balance:	\$ 525,037

Project Details: This project will provide for capital improvements for existing North Oconee River Greenway Trail sections in the general vicinity of the Sandy Creek Nature Center and Walker Hall.

Project Update: On July 16, 2024, M&C approved a construction services contract to Structural Resources, Inc. in the amount of \$1,540,343.00. Construction services contract has been executed with work scheduled to begin in September 2024 and anticipated to be completed by April, 2025. Construction in progress.

26 - W. Broad & Hancock Neighborhood Pedestrian Improvements Program

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$1,470,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 132,398 9%		
Available Balance:	\$1,337,602		

Project Details: This program includes capital improvements for bicycle and pedestrian safety improvements within the general vicinities of the W. Broad Area and Hancock neighborhoods.

Project Update: Held User Group kickoff meeting on December 4, 2024. Staff is working with the User Group to identify ideas for project concept. The User Group continues to meet monthly.

27 - Five Points Intersection Safety Improvements Project

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning
Original Budget:	\$1,470,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 58,179 4%		
Available Balance:	\$1,411,821		

Project Details: This project includes capital improvements for safety improvements at the Five Points Intersection.

Project Update: The User Group kick-off meeting was held on October 16, 2024. Staff presented some preliminary ideas for intersection improvements. The User Group continues to meet monthly

28 - College Square Pedestrian Plaza Project

Project Manager:	Diana Jackson	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Planning

Original Budget:	\$490,000
Budget Amendments:	\$ 0
Grants:	\$ 0
Expenses, Encumbered, Designated:	\$ 77,000 16%
Available Balance:	\$413,000

Project Details: This project will provide pedestrian improvements to the College Square Plaza area. The project funding is only adequate to provide some initial design, planning, public engagement, and project management. Majority of any construction funding will have to be provided by other sources.

Project Update: M&C approved the Proposed Project Concept at the October 01, 2024 Regular Session. Public engagement has begun and will run through January 3, 2025. The Intergovernmental Agreement with the Athens Downtown Development Authority is up for approval at the November 6, 2024 Regular Session. ADDA and their Designers held four public engagement meetings with targeted stakeholders and two public meetings. [An online survey closed on January 3, 2025. Almost 1300 visit were made to the survey site. 600 surveys were completed along with 62 open ended feedback received. Designers will be sorting through these comments and sharing them with the M&C.](#)

30 - Traffic Safety Infrastructure Improvements Program

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Preliminary Design
Original Budget:	\$2,940,000		
Budget Amendments	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered, Designated:	\$ 309,236 11%		
Available Balance:	\$2,630,764		

Project Details: This program provides funding for capital improvements for traffic safety infrastructure improvements.

Project Update: On April 2, 2024, M&C approved the Project Concept Traffic Signal Evaluation Criteria for the Traffic Safety Infrastructure program. This criteria includes 5 attributes that includes Signal Infrastructure Condition, Recent Maintenance Efforts, Meeting Industry Standards, Traffic Volumes, and Previous 5-yr crash data. Internal ACC design Team Kick-off meeting was held on May 30, 2024 and survey efforts are in-progress. Preliminary Design efforts are in progress for the following 5 intersections: College Ave/Dr MLK Jr Pkwy/Ruth St, Cherokee Road/Buddy Christian Way/Beaverdam Rd, Baldwin Street/Jackson Street, Hawthorne Avenue/Old Epps Bridge Rd., and Hawthorne Ave/Old West Broad St. On December 3, 2024 at the regular season meeting M&C approved the proposed Preliminary Design.

32 - Traffic Signage Replacement Program

Project Manager:	Derek Doster	Project Sponsor:	Transportation and Public Works
Project Status:	Active	Current Phase:	Preliminary Design
Original Budget:	\$735,000		
Budget Amendments:	\$ 0		
Grants:	\$ 0		
Expenses, Encumbered Designated:	\$113,000 15%		
Available Balance:	\$622,000		

Project Details: This program provides funding for capital improvements for traffic signage infrastructure improvements.

Project Update: On June 5, 2024, M&C approved the Project Concept for the Traffic Signage Replacement Program. This action included approval of 15 inventory attributes that include items such as type of sign, physical condition of sign, and support structure of sign. Sign inventory is in progress.



TSPLOST 2018 Revenue Status - Summary

For Period Ending 1/1/2025

Fiscal Year	Month	Year	Projected Revenue	Actual Revenue	Revenue Variance	Earned Interest	Total Revenue
Totals - FY 2018 (Tier 1) TSPLOST 2018			\$5,300,001	\$5,194,686	(\$105,315)	\$24,241	\$5,218,928
Totals - FY 2019 (Tier 2) TSPLOST 2018			\$21,200,004	\$22,938,093	\$1,738,089	\$360,731	\$23,298,824
Totals - FY 2020 (Tier 3) TSPLOST 2018			\$21,800,004	\$24,596,470	\$2,796,466	\$406,491	\$25,002,961
Totals - FY 2021 (Tier 4) TSPLOST 2018			\$22,200,000	\$26,119,114	\$3,919,114	\$33,841	\$26,152,955
Totals - FY 2022 (Tier 5) TSPLOST 2018			\$22,700,004	\$30,188,231	\$7,488,227	\$142,617	\$30,330,848
Totals - FY 2023 (Tier 6) TSPLOST 2018			\$16,299,987	\$8,063,269	(\$8,236,718)	\$2,461,346	\$10,524,615
Totals - FY 2024 (Tier 7) TSPLOST 2018			\$0	\$0	\$0	\$2,967,008	\$2,967,008
FY 2025 (Tier 8) TSPLOST 2018							
	July	2024	\$0	\$0	\$0	\$239,012	\$239,012
	August	2024	\$0	\$0	\$0	\$239,281	\$239,281
	September	2024	\$0	\$0	\$0	\$223,506	\$223,506
	October	2024	\$0	\$0	\$0	\$215,945	\$215,945
	November	2024	\$0	\$0	\$0	\$202,777	\$202,777
	December	2024	\$0	\$0	\$0	\$0	\$0
Totals - FY 2025 (Tier 8) TSPLOST 2018			\$0	\$0	\$0	\$1,120,520	\$1,120,520
Program Totals -			\$109,500,000	\$117,099,863	\$7,599,863	\$7,516,797	\$124,616,660



TSPLOST 2018 Expenditure Summary

For Period Ending 1/1/2025

Project Information	Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
00 - TSPLOST 2018 Program Management Project Manager: Keith Sanders	TSPLOST 2018	\$3,247,800	\$592,725	\$0	\$1,241,203	\$1,413,873
Project Sponsor: SPLOST	Total	\$3,247,800	\$592,725	\$0	\$1,241,203	\$1,413,873
01 - Airport CIP Matching Funds Project Manager: Keith Sanders	Grant Funds	\$12,793,021	\$0	\$0	\$12,793,021	\$0
Project Sponsor: Athens-Ben Epps Airport	TSPLOST 2018	\$2,069,642	\$13,778	\$0	\$1,456,434	\$599,430
	Total	\$14,862,663	\$13,778	\$0	\$14,249,455	\$599,431
02 - Transit Vehicle Expansion & Replacement Project Project Manager: Keith Sanders	Grant Funds	\$4,059,930	\$0	\$0	\$4,059,930	\$0
Project Sponsor: Transit	TSPLOST 2018	\$3,920,191	\$238,523	\$0	\$456,203	\$3,225,464
	Total	\$7,980,121	\$238,523	\$0	\$4,516,133	\$3,225,464
03 - Bus Stop & Transfer Facilities Improvements Project Manager: Keith Sanders	Grant Funds	\$585,874	\$427,520	\$0	\$158,354	\$0
Project Sponsor: Transit	TSPLOST 2018	\$537,339	\$140,386	\$40,890	\$111,310	\$244,754
	Total	\$1,123,213	\$567,906	\$40,890	\$269,663	\$244,754
04 - Transit Service Expansion Project Manager: Keith Sanders	TSPLOST 2018	\$851,966	\$0	\$0	\$851,965	\$1
Project Sponsor: Transit	Total	\$851,966	\$0	\$0	\$851,965	\$1
05 - Oconee Rivers Greenway Program Project Manager: Derek Doster	Other Funds	\$10,600	\$10,600	\$0	\$0	\$0
Project Sponsor: Leisure Services	TSPLOST 2018	\$9,650,745	\$1,592,681	\$141,846	\$7,788,046	\$128,171
	Revenue Variance - TS	\$700,000	\$0	\$0	\$411,870	\$288,130
	Total	\$10,361,345	\$1,603,281	\$141,846	\$8,199,917	\$416,301
06 - Firefly Trail Project Manager: Derek Doster	Donated Funds	\$51,395	\$0	\$0	\$51,395	\$0
Project Sponsor: Leisure Services	TSPLOST 2018	\$16,166,455	\$736,458	\$175,850	\$15,021,043	\$233,103
	Revenue Variance - TS	\$2,500,000	\$2,500,000	\$0	\$0	\$0
	Total	\$18,717,850	\$3,236,458	\$175,850	\$15,072,438	\$233,103
07 - Pavement Rehabilitation Project Manager: Derek Doster	TSPLOST 2018	\$24,546,646	\$0	\$133,140	\$24,413,506	\$0
Project Sponsor: Transportation & Public Works	TSPLOST 2018 Interest	\$159,000	\$0	\$0	\$159,000	\$0
	Total	\$24,705,646	\$0	\$133,140	\$24,572,506	\$0

Project Information		Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
08 - Tallassee Road Bridge Replacement Project Manager: Rani Katreeb	Grant Funds	\$750,000	\$0	\$0	\$750,000	\$0	
	Other Funds	\$490,554	\$0	\$0	\$490,554	\$0	
	TSPLOST 2018	\$6,388,808	\$96,942	\$129,411	\$5,735,147	\$427,308	
	TSPLOST 2018 Interest	\$676,000	\$0	\$16,750	\$659,250	\$0	
Project Sponsor: Transportation & Public Works	Total	\$8,305,362	\$96,942	\$146,161	\$7,634,951	\$427,308	
09 - Downtown Transportation Improvement Program Project Manager: Diana Jackson	TSPLOST 2018	\$6,355,688	\$105,886	\$147,703	\$3,525,098	\$2,577,001	
	Total	\$6,355,688	\$105,886	\$147,703	\$3,525,098	\$2,577,001	
	Total	\$6,355,688	\$105,886	\$147,703	\$3,525,098	\$2,577,001	
10 - Whitehall Road at South Milledge Avenue Project Manager: Derek Doster	TSPLOST 2018	\$2,222,432	\$0	\$0	\$2,222,432	\$0	
	Total	\$2,222,432	\$0	\$0	\$2,222,432	\$0	
	Total	\$2,222,432	\$0	\$0	\$2,222,432	\$0	
11 - Bicycle Improvement Program Project Manager: Diana Jackson	TSPLOST 2018	\$5,790,554	\$4,343,359	\$170,999	\$634,913	\$641,283	
	Revenue Variance - TS	\$948,912	\$948,912	\$0	\$0	\$0	
	Total	\$6,739,466	\$5,292,271	\$170,999	\$634,913	\$641,283	
12 - Pedestrian Improvement Program Project Manager: Diana Jackson	TSPLOST 2018	\$10,207,391	\$5,783,061	\$269,511	\$1,414,647	\$2,740,172	
	Revenue Variance - TS	\$948,912	\$948,912	\$0	\$0	\$0	
	Total	\$11,156,303	\$6,731,973	\$269,511	\$1,414,647	\$2,740,172	
13 - W Broad Pedestrian Improvements Project Manager: Keith Sanders	TSPLOST 2018	\$3,860,191	\$102,597	\$224,774	\$1,262,802	\$2,270,019	
	Total	\$3,860,191	\$102,597	\$224,774	\$1,262,802	\$2,270,019	
	Total	\$3,860,191	\$102,597	\$224,774	\$1,262,802	\$2,270,019	
14 - Lexington Highway Corridor Improvements Project Manager: Derek Doster	TSPLOST 2018	\$3,860,191	\$29,932	\$195,132	\$643,948	\$2,991,179	
	Total	\$3,860,191	\$29,932	\$195,132	\$643,948	\$2,991,179	
	Total	\$3,860,191	\$29,932	\$195,132	\$643,948	\$2,991,179	
15 - Atlanta Highway Corridor Improvements Project Manager: Derek Doster	TSPLOST 2018	\$3,860,191	\$2,052,536	\$30,185	\$1,068,202	\$709,268	
	Total	\$3,860,191	\$2,052,536	\$30,185	\$1,068,202	\$709,268	
	Total	\$3,860,191	\$2,052,536	\$30,185	\$1,068,202	\$709,268	
16 - Prince Avenue Corridor Improvements Project Manager: Diana Jackson	TSPLOST 2018	\$3,860,191	\$64,500	\$149,000	\$1,150,988	\$2,495,703	
	Total	\$3,860,191	\$64,500	\$149,000	\$1,150,988	\$2,495,703	
	Total	\$3,860,191	\$64,500	\$149,000	\$1,150,988	\$2,495,703	
17 - Acquisition of Private Streets Project Manager: Keith Sanders	TSPLOST 2018	\$328,194	\$1,846	\$0	\$76,063	\$250,285	
	Total	\$328,194	\$1,846	\$0	\$76,063	\$250,285	
Project Sponsor: Transportation & Public Works	Total	\$328,194	\$1,846	\$0	\$76,063	\$250,285	

Project Information	Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
18 - City of Winterville Project Manager: Ashley Barnett	TSPLOST 2018	\$969,000	\$0	\$0	\$969,000	\$0
Project Sponsor: City of Winterville	Total	\$969,000	\$0	\$0	\$969,000	\$0
19 - Town of Bogart Project Manager: Ashley Barnett	TSPLOST 2018	\$271,000	\$0	\$0	\$271,000	\$0
Project Sponsor: Town of Bogart	Total	\$271,000	\$0	\$0	\$271,000	\$0
	Total	\$133,638,813	\$20,731,155	\$1,825,191	\$89,847,323	\$21,235,143

Unallocated Interest	\$6,681,797
Unallocated Revenue	\$2,502,039

Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
TSPLOST 2018	\$108,964,615	\$15,895,211	\$1,808,441	\$70,313,950	\$20,947,013
Grant Funds	\$18,188,824	\$427,520	\$0	\$17,761,304	\$0
Revenue Variance - TSPLOST 2018	\$5,097,824	\$4,397,824	\$0	\$411,870	\$288,130
Other Funds	\$501,154	\$10,600	\$0	\$490,554	\$0
Donated Funds	\$51,395	\$0	\$0	\$51,395	\$0
TSPLOST 2018 Interest	\$835,000	\$0	\$16,750	\$818,250	\$0
Grand Total	\$133,638,813	\$20,731,155	\$1,825,191	\$89,847,323	\$21,235,143



TSPLST 2023 Revenue Status - Summary

For Period Ending 1/1/2025

Fiscal Year	Month	Year	Projected Revenue	Actual Revenue	Revenue Variance	Earned Interest	Total Revenue
Totals - FY 2023 (Tier 1) TSPLST 2023			\$21,508,002	\$25,053,180	\$3,545,178	\$315,239	\$25,368,420
Totals - FY 2024 (Tier 2) TSPLST 2023			\$29,105,004	\$35,010,966	\$5,905,962	\$1,918,945	\$36,929,911
FY 2025 (Tier 3) TSPLST 2023							
	July	2024	\$2,473,917	\$2,846,628	\$372,711	\$229,984	\$3,076,611
	August	2024	\$2,473,917	\$3,047,342	\$573,425	\$243,634	\$3,290,976
	September	2024	\$2,473,917	\$2,992,127	\$518,210	\$235,469	\$3,227,596
	October	2024	\$2,473,917	\$3,183,247	\$709,330	\$237,093	\$3,420,340
	November	2024	\$2,473,917	\$3,094,204	\$620,287	\$235,611	\$3,329,815
	December	2024	\$0	\$0	\$0	\$0	\$0
Totals - FY 2025 (Tier 3) TSPLST 2023			\$12,369,585	\$15,163,548	\$2,793,963	\$1,181,791	\$16,345,339
Program Totals -			\$62,982,591	\$75,227,695	12,245,104	3,415,975	\$78,643,670



SPLOST 2023 Expenditure Summary

For Period Ending 1/1/2025

Project Information	Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
00 - SPLOST 2023 Program Management	General Funds	\$56,973	\$0	\$0	\$24,388	\$32,585
Project Manager: Josh Hawkins	SPLOST 2023	\$2,966,000	\$0	\$0	\$404,259	\$2,561,741
Project Sponsor: SPLOST	Total	\$3,022,973	\$0	\$0	\$428,647	\$2,594,326
01 - Winterville Storm Drainage Improvements Project	SPLOST 2023	\$2,712,000	\$35,680	\$7,831	\$383,706	\$2,284,783
Project Manager: Mike Kajder						
Project Sponsor: Transportation & Public Works	Total	\$2,712,000	\$35,680	\$7,831	\$383,706	\$2,284,783
02 - Lexington Road Corridor Improvements Project	SPLOST 2023	\$8,190,000	\$586,252	\$136,349	\$83,059	\$7,384,340
Project Manager: Derek Doster						
Project Sponsor: Transportation & Public Works	Total	\$8,190,000	\$586,252	\$136,349	\$83,059	\$7,384,340
03 - Lexington Road Sidewalk Gap Project	SPLOST 2023	\$532,000	\$3,937	\$5,386	\$68,053	\$454,624
Project Manager: Derek Doster						
Project Sponsor: Transportation & Public Works	Total	\$532,000	\$3,937	\$5,386	\$68,053	\$454,624
04 - Atlanta Highway/W. Broad Street Improvements Project	SPLOST 2023	\$8,722,000	\$405,827	\$0	\$173	\$8,316,000
Project Manager: Derek Doster						
Project Sponsor: Transportation & Public Works	Total	\$8,722,000	\$405,827	\$0	\$173	\$8,316,000
05 - Prince Avenue/Jefferson Road Corridor Improvements Project	SPLOST 2023	\$8,722,000	\$386,416	\$31,940	\$82,204	\$8,221,440
Project Manager: Diana Jackson						
Project Sponsor: Transportation & Public Works	Total	\$8,722,000	\$386,416	\$31,940	\$82,204	\$8,221,440
06 - Timothy Rd/Mitchell Bridge Rd Corridor Improvements Project	SPLOST 2023	\$6,762,000	\$358,303	\$0	\$16,823	\$6,386,873
Project Manager: Derek Doster						
Project Sponsor: Transportation & Public Works	Total	\$6,762,000	\$358,303	\$0	\$16,823	\$6,386,873
07 - E. Athens Neighborhood Pedestrian and Lighting Safety Project	SPLOST 2023	\$37,000	\$2,972	\$0	\$1,028	\$33,000
Project Manager: Keith Sanders						
Project Sponsor: Transportation & Public Works	Total	\$37,000	\$2,972	\$0	\$1,028	\$33,000

Project Information	Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
08 - E. Athens Neighborhood Traffic Management Program Project Manager: Keith Sanders	TSPLOST 2023	\$141,000	\$11,972	\$0	\$1,028	\$128,000
Project Sponsor: Transportation & Public Works	Total	\$141,000	\$11,972	\$0	\$1,028	\$128,000
09 - E. Athens Neighborhood Pavement Rehabilitation Project Project Manager: Keith Sanders	TSPLOST 2023	\$239,000	\$18,860	\$0	\$1,274	\$218,866
Project Sponsor: Transportation & Public Works	Total	\$239,000	\$18,860	\$0	\$1,274	\$218,866
10 - E. Athens Neighborhood Transit Improvements Project Project Manager: Keith Sanders	TSPLOST 2023	\$354,000	\$30,972	\$0	\$1,028	\$322,000
Project Sponsor: Transit	Total	\$354,000	\$30,972	\$0	\$1,028	\$322,000
11 - E. Athens MLK Parkway Oconee River Greenway Trail Project Project Manager: Derek Doster	TSPLOST 2023	\$2,408,000	\$171,339	\$7,084	\$45,340	\$2,184,237
Project Sponsor: Leisure Services	Total	\$2,408,000	\$171,339	\$7,084	\$45,340	\$2,184,237
12 - E. Athens Neighborhood Greenway Connectors Project Project Manager: Keith Sanders	TSPLOST 2023	\$1,496,000	\$206,972	\$0	\$1,028	\$1,288,000
Project Sponsor: Leisure Services	Total	\$1,496,000	\$206,972	\$0	\$1,028	\$1,288,000
13 - E. Athens Neighborhood Pedestrian Improvements Project Project Manager: Keith Sanders	TSPLOST 2023	\$971,000	\$90,972	\$0	\$1,028	\$879,000
Project Sponsor: Transportation & Public Works	Total	\$971,000	\$90,972	\$0	\$1,028	\$879,000
14 - N. Athens Transportation Improvements Program Project Manager: Keith Sanders	TSPLOST 2023	\$8,041,000	\$330,972	\$0	\$1,028	\$7,709,000
Project Sponsor: Transportation & Public Works	Total	\$8,041,000	\$330,972	\$0	\$1,028	\$7,709,000
15 - Sycamore Drive Pedestrian Improvements Project Project Manager: Mike Kajder	TSPLOST 2023	\$1,470,000	\$73,190	\$77,309	\$19,831	\$1,299,670
Project Sponsor: Transportation & Public Works	Total	\$1,470,000	\$73,190	\$77,309	\$19,831	\$1,299,670
16 - Westchester Neighborhood Area Pedestrian Improvements Project Project Manager: Mike Kajder	TSPLOST 2023	\$1,892,000	\$75,313	\$108,805	\$8,001	\$1,699,880
Project Sponsor: Transportation & Public Works	Total	\$1,892,000	\$75,313	\$108,805	\$8,001	\$1,699,880

Project Information	Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
17 - Stonehenge Neighborhood Area Pedestrian Improvements Project Project Manager: Daniel Garren	TSPLOST 2023	\$4,612,000	\$188,872	\$0	\$8,128	\$4,415,000
Project Sponsor: Transportation & Public Works	Total	\$4,612,000	\$188,872	\$0	\$8,128	\$4,415,000
18 - Firefly Trail Project Project Manager: Derek Doster	TSPLOST 2023	\$7,345,000	\$267,710	\$55,390	\$18,744	\$7,003,156
Project Sponsor: Leisure Services	Total	\$7,345,000	\$267,710	\$55,390	\$18,744	\$7,003,156
19 - ACCGov Public Transit Operations Program Project Manager: Keith Sanders	TSPLOST 2023	\$27,347,000	\$57,000	\$0	\$3,493,522	\$23,796,478
Project Sponsor: Transit	Total	\$27,347,000	\$57,000	\$0	\$3,493,522	\$23,796,478
20 - Electrify the Fleet Program Project Manager: Josh Hawkins	TSPLOST 2023	\$8,403,000	\$115,000	\$0	\$0	\$8,288,000
Project Sponsor: Central Services	Total	\$8,403,000	\$115,000	\$0	\$0	\$8,288,000
21 - Pavement and Bridge Maintenance Program Project Manager: Stephen Bailey	General Funds	\$2,372,895	\$0	\$0	\$2,372,895	\$0
	Grant Funds	\$3,620,337	\$648,000	\$554,158	\$2,418,179	\$0
	Other Funds	\$152,152	\$0	\$152,152	\$0	\$0
	ARPA Funds	\$61,124	\$0	\$61,124	\$0	\$0
	TSPLOST 2023	\$21,751,000	\$414,408	\$3,990,403	\$6,803,884	\$10,542,306
Project Sponsor: Transportation & Public Works	Total	\$27,957,508	\$1,062,408	\$4,757,837	\$11,594,957	\$10,542,306
22 - Culvert and Live Stream Pipe Replacement Program Project Manager: Stephen Bailey	TSPLOST 2023	\$3,487,000	\$43,149	\$0	\$1,155,333	\$2,288,518
Project Sponsor: Transportation & Public Works	Total	\$3,487,000	\$43,149	\$0	\$1,155,333	\$2,288,518
23 - Greenway Trail Safety Improvements Project Project Manager: Mike Kajder	TSPLOST 2023	\$1,693,000	\$29,182	\$907,202	\$231,579	\$525,037
Project Sponsor: Leisure Services	Interest	\$750,000	\$0	\$750,000	\$0	\$0
	Total	\$2,443,000	\$29,182	\$1,657,202	\$231,579	\$525,037
24 - Historic District Street Signage Project Project Manager: Tim Griffeth	TSPLOST 2023	\$93,000	\$1,818	\$0	\$182	\$91,000
Project Sponsor: Transportation & Public Works	Total	\$93,000	\$1,818	\$0	\$182	\$91,000
25 - Bicycle and Pedestrian Improvements Program Project Manager: Stephen Bailey	TSPLOST 2023	\$7,840,000	\$521,637	\$0	\$363	\$7,318,000
Project Sponsor: Transportation & Public Works	Total	\$7,840,000	\$521,637	\$0	\$363	\$7,318,000

Project Information	Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
26 - W. Broad & Hancock Neighborhood Pedestrian Improvements Program Project Manager: Diana Jackson	TSPLOST 2023	\$1,470,000	\$131,950	\$0	\$448	\$1,337,602
Project Sponsor: Transportation & Public Works	Total	\$1,470,000	\$131,950	\$0	\$448	\$1,337,602
27 - Five Points Intersection Safety Improvements Project Project Manager: Riki Forney	TSPLOST 2023	\$1,470,000	\$56,255	\$0	\$1,924	\$1,411,821
Project Sponsor: Transportation & Public Works	Total	\$1,470,000	\$56,255	\$0	\$1,924	\$1,411,821
28 - College Square Pedestrian Plaza Project Project Manager: Diana Jackson	TSPLOST 2023	\$490,000	\$70,396	\$0	\$6,604	\$413,000
Project Sponsor: Transportation & Public Works	Total	\$490,000	\$70,396	\$0	\$6,604	\$413,000
29 - Smithonia & Smokey Road Pedestrian Improvements Project Project Manager: Mike Kajder	TSPLOST 2023	\$1,916,000	\$81,911	\$0	\$89	\$1,834,000
Project Sponsor: Transportation & Public Works	Total	\$1,916,000	\$81,911	\$0	\$89	\$1,834,000
30 - Traffic Safety Infrastructure Improvements Program Project Manager: Derek Doster	TSPLOST 2023	\$2,940,000	\$102,702	\$81,209	\$125,325	\$2,630,764
Project Sponsor: Transportation & Public Works	Total	\$2,940,000	\$102,702	\$81,209	\$125,325	\$2,630,764
31 - Neighborhood Traffic Management Program Project Manager: Stephen Bailey	General Funds	\$245,605	\$0	\$0	\$245,605	\$0
Project Sponsor: Transportation & Public Works	TSPLOST 2023	\$980,000	\$26,911	\$100,000	\$40,024	\$813,065
Project Sponsor: Transportation & Public Works	Total	\$1,225,605	\$26,911	\$100,000	\$285,629	\$813,065
32 - Traffic Signage Replacement Program Project Manager: Derek Doster	TSPLOST 2023	\$735,000	\$5,506	\$38,400	\$69,094	\$622,000
Project Sponsor: Transportation & Public Works	Total	\$735,000	\$5,506	\$38,400	\$69,094	\$622,000
33 - City of Winterville Project Manager: Ashley Barnett	TSPLOST 2023	\$1,429,000	\$1,143,200	\$0	\$285,800	\$0
Project Sponsor: City of Winterville	Total	\$1,429,000	\$1,143,200	\$0	\$285,800	\$0
34 - Town of Bogart Project Manager: Ashley Barnett	TSPLOST 2023	\$344,000	\$275,200	\$0	\$68,800	\$0
Project Sponsor: Town of Bogart	Total	\$344,000	\$275,200	\$0	\$68,800	\$0
	Total	\$157,259,086	\$6,970,759	\$7,064,741	\$18,489,800	\$124,733,787

Unallocated Interest	\$2,665,975
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Fund Source	Budgeted	Designated	Encumbered	Expenses	Available Balance
TSPLOST 2023	\$150,000,000	\$6,322,759	\$5,547,306	\$13,428,733	\$124,701,202
General Funds	\$2,675,473	\$0	\$0	\$2,642,888	\$32,585
Grant Funds	\$3,620,337	\$648,000	\$554,158	\$2,418,179	\$0
Other Funds	\$152,152	\$0	\$152,152	\$0	\$0
ARPA Funds	\$61,124	\$0	\$61,124	\$0	\$0
TSPLOST 2023 Interest	\$750,000	\$0	\$750,000	\$0	\$0
Grand Total	\$157,259,086	\$6,970,759	\$7,064,741	\$18,489,800	\$124,733,787