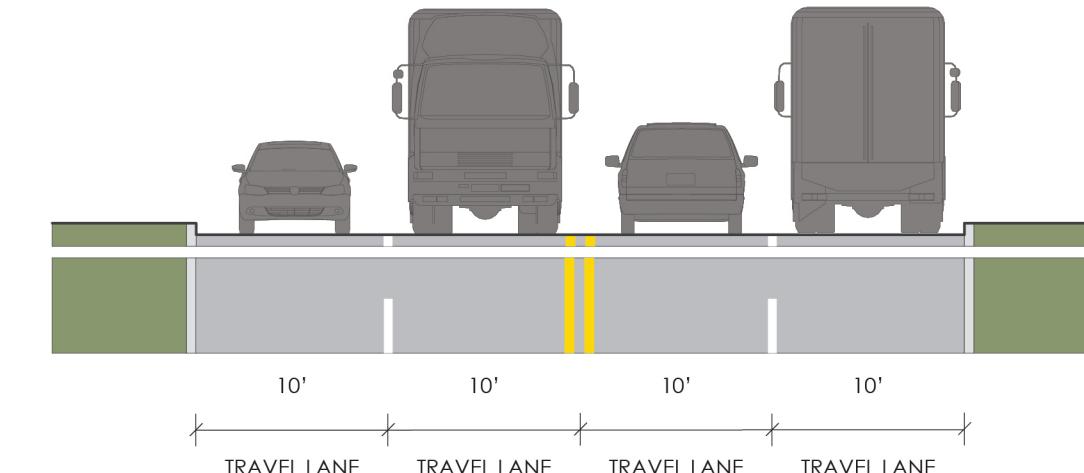


NORTH CHASE STREET

BARBER STREET TO LOOP 10

Existing Conditions

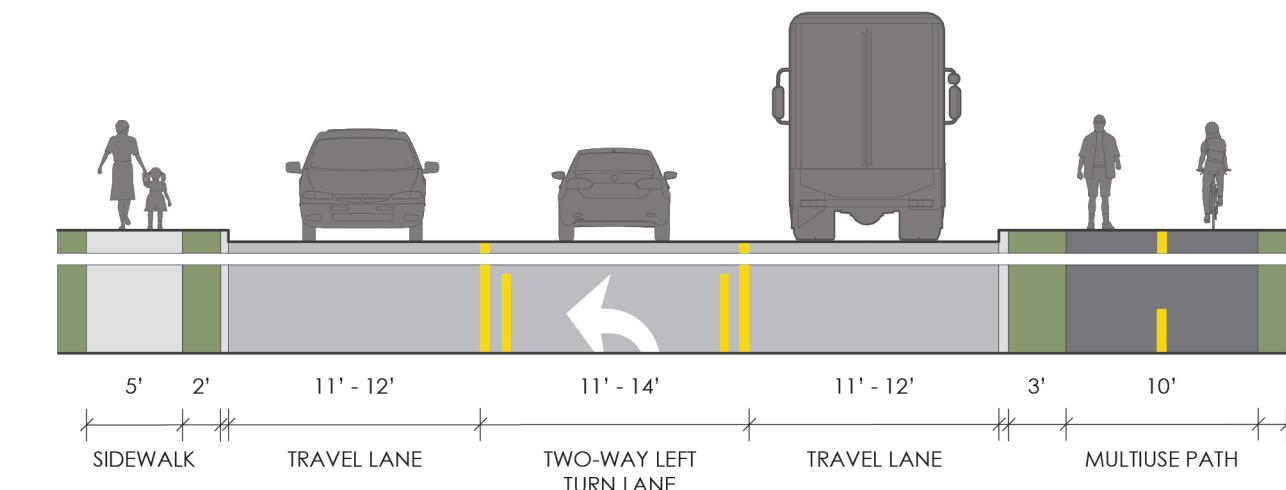
- The existing typical section along North Chase Street from Barber Street to the Loop 10 interchange has four 10' wide travel lanes with two lanes in each direction.
- The existing curb-to-curb distance is 40' wide.
- There are no existing sidewalks or bicycle facilities.



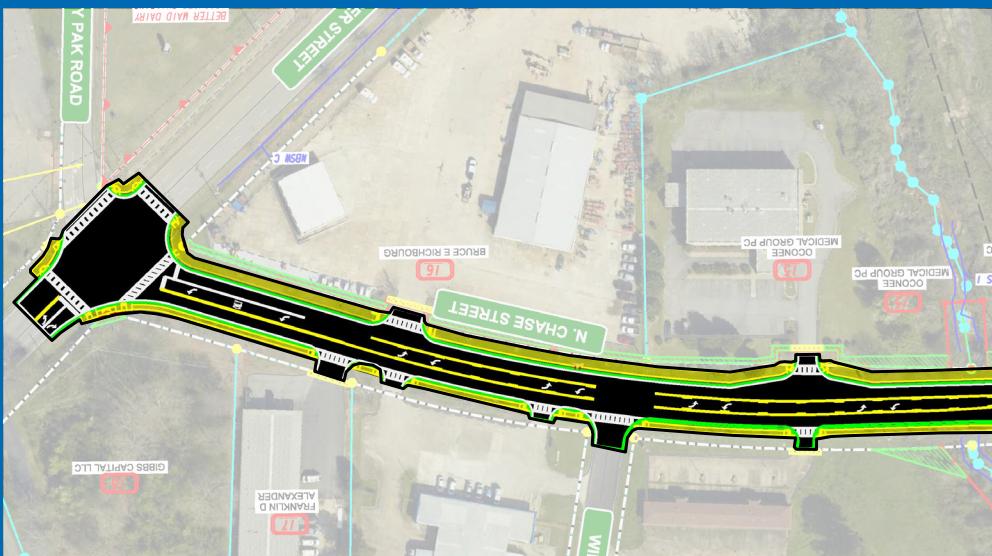
Proposed Typical Section

- The proposed typical section creates a lane diet with two travel lanes and a two-way center left turn lane.
- The proposed curb-to-curb width is reduced to approximately 38'.
- A multiuse path is proposed on the eastern side of the roadway, and a sidewalk is proposed on the western side of the roadway.

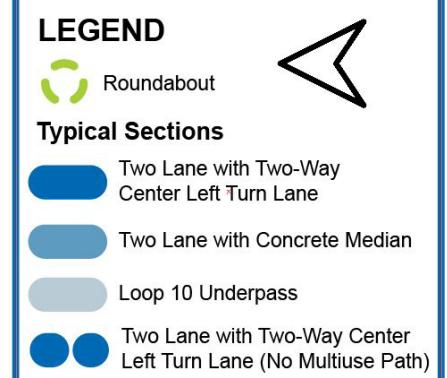
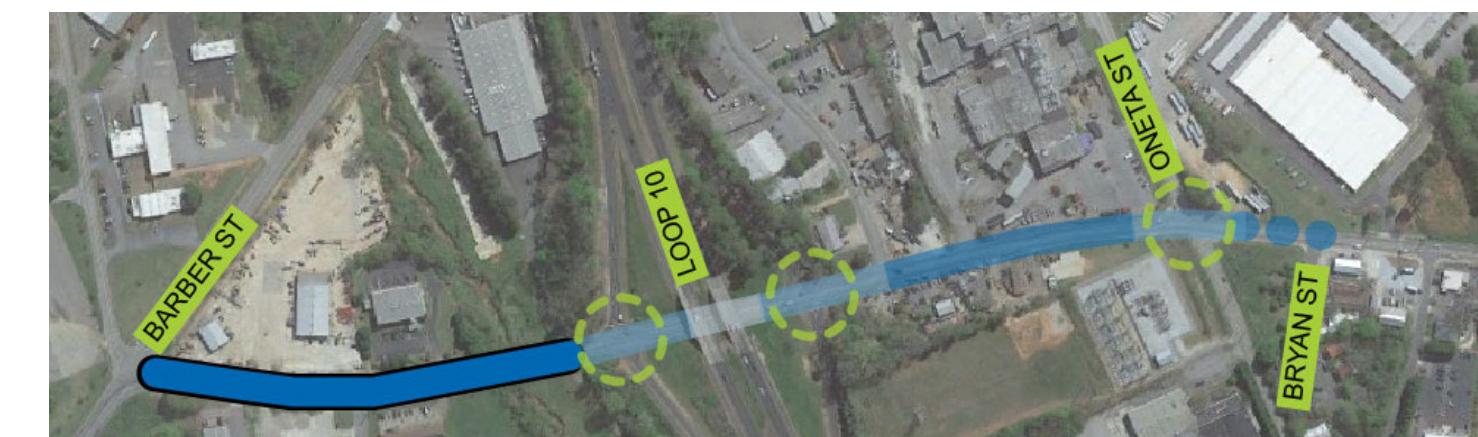
TYPICAL SECTION PROPOSED



Schematic Design



CONTEXT MAP

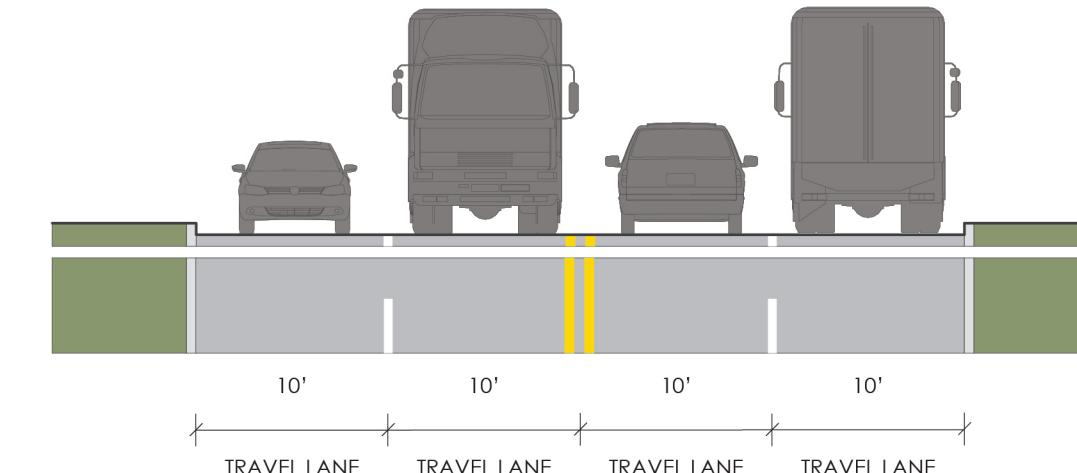


NORTH CHASE STREET

LOOP 10 INTERCHANGE APPROACHES

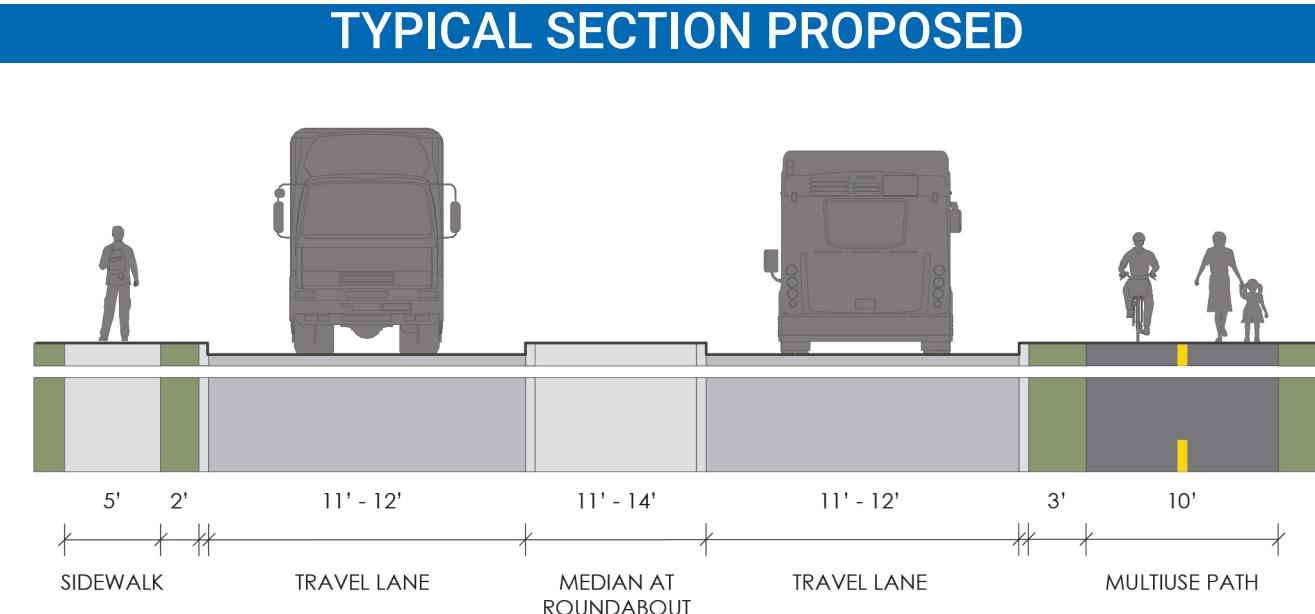
Existing Conditions

- The existing typical along North Chase Street from Barber Street to the Loop 10 Interchange has four 10' wide travel lanes with two lanes going in each direction.
- The existing curb-to-curb distance is 40' wide.
- There are no existing sidewalks or bicycle facilities.



Proposed Typical Section

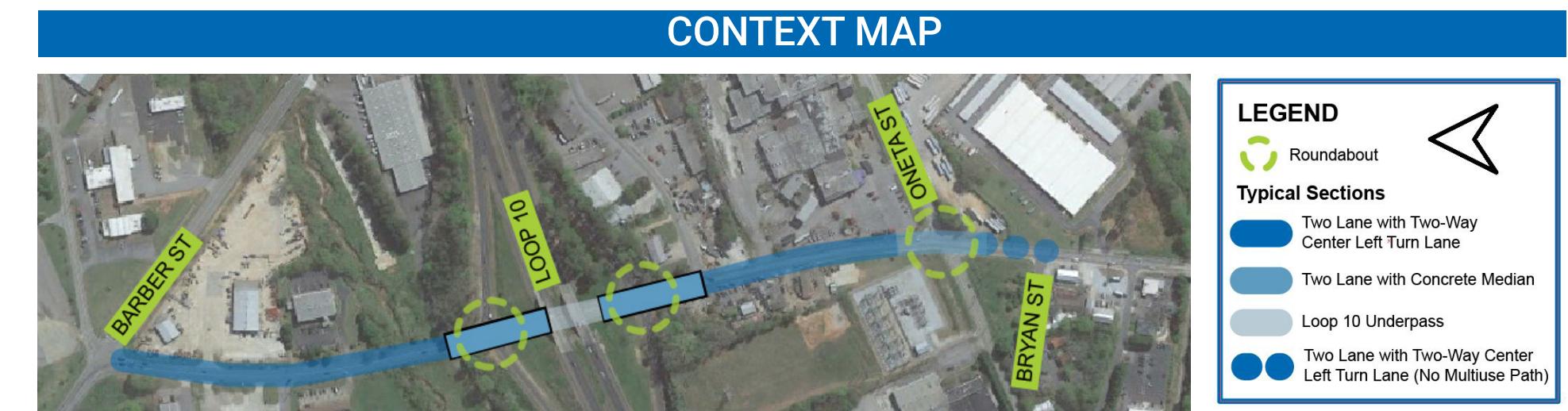
- The proposed typical section reduces the lanes down to one in each direction with a concrete median inbetween.
- The 10' multiuse path is maintained on the eastern side of the roadway through the roundabouts with ADA compliant sidewalks and traffic calming that prioritizes pedestrians and bicyclists.
- A sidewalk is proposed on the western side of the roadway.



Schematic Design



TYPICAL SECTION EXISTING



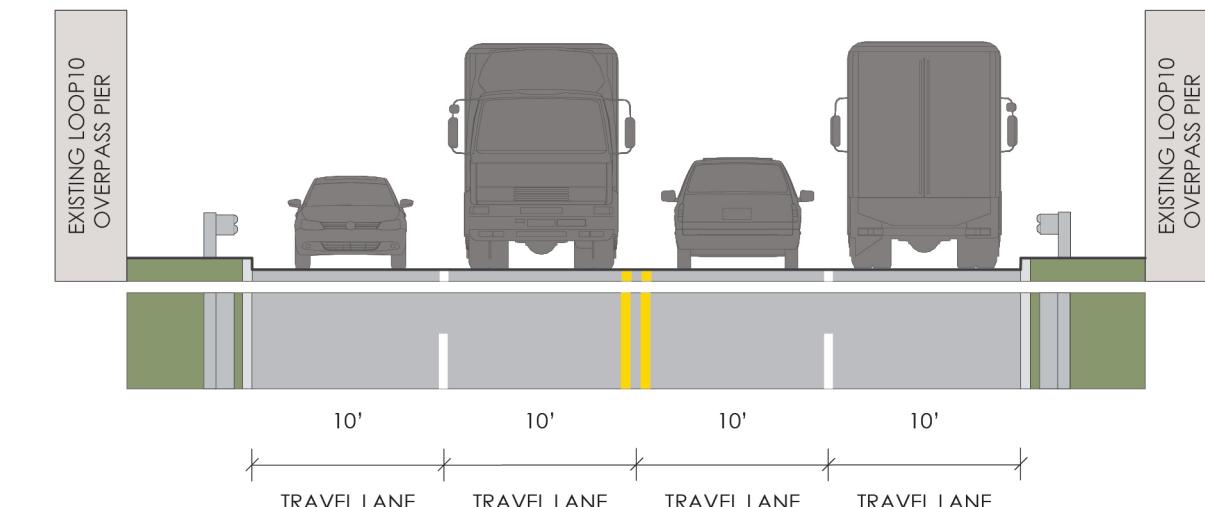
NORTH CHASE STREET

LOOP 10 UNDERPASS

Existing Conditions

- The existing typical along North Chase Street underneath the Loop 10 bridge are four 10' wide travel lanes with two in each direction.
- The existing curb-to-curb distance is 40' wide.
- There are no existing sidewalks or bicycle facilities.

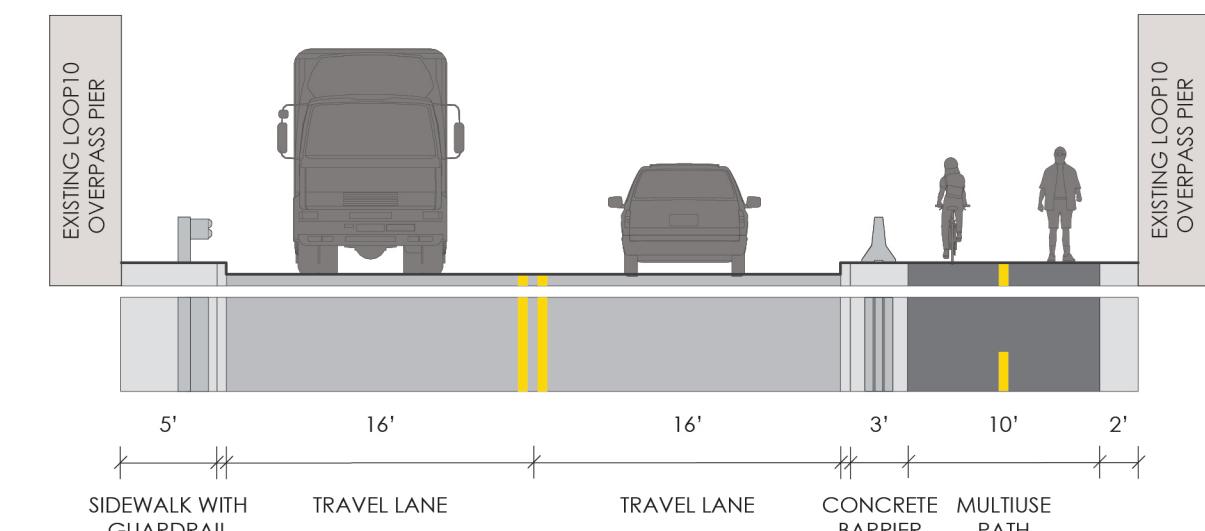
TYPICAL SECTION EXISTING



Proposed Typical Section

- The width of the road and number of travel lanes is reduced to two 16' wide lanes with one in each direction.
- A 10' wide multiuse path that is protected with concrete barriers is proposed on the eastern side of the roadway.
- The sidewalk is maintained on the western side of the roadway. However, this sidewalk is narrow due to constraints with existing overpass piers. Pedestrians on the western side of the roadway may choose to use new crosswalks at the roundabouts to use the multiuse path.

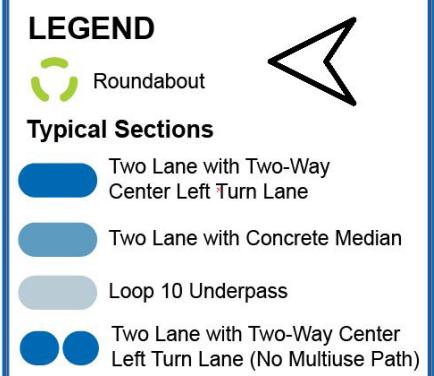
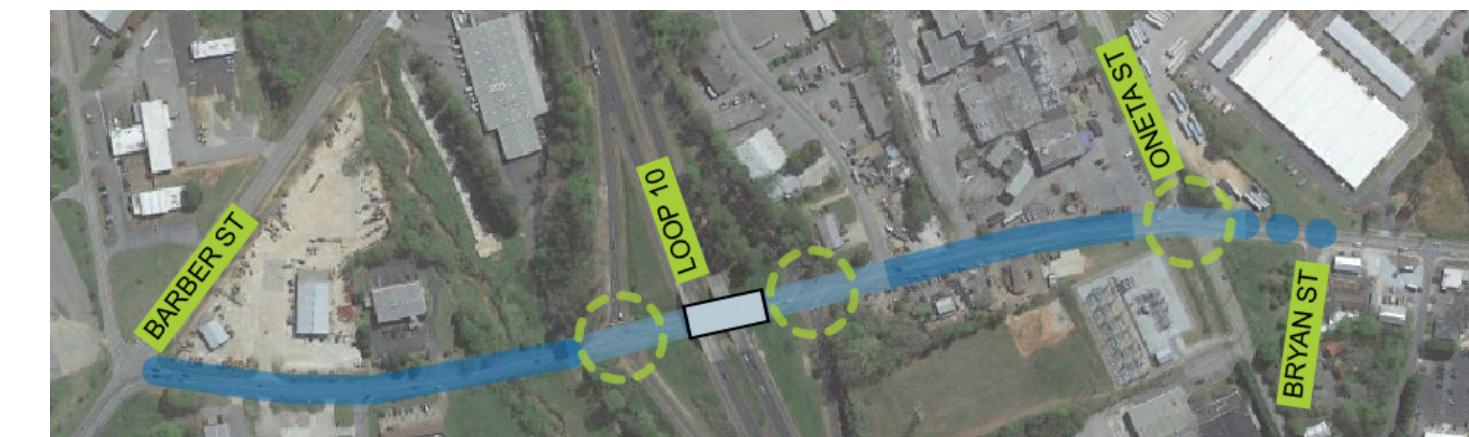
TYPICAL SECTION PROPOSED



Schematic Design



CONTEXT MAP



NORTH CHASE STREET

LOOP 10 TO ONETA STREET

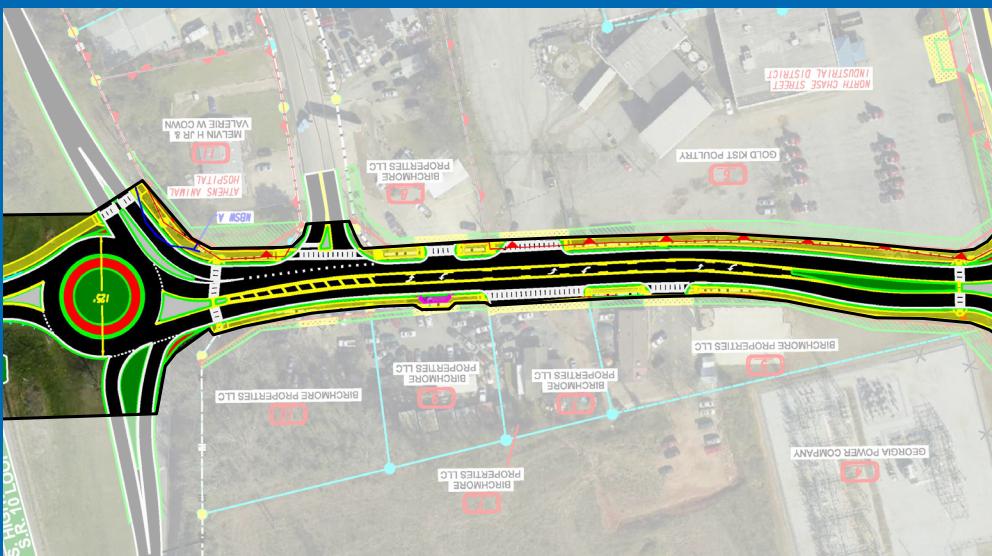
Existing Conditions

- The existing typical section is two 10.5' – 11' wide travel lanes, one in each direction, with a 10.5' – 11' wide two-way center left turn lane.
- The existing curb-to-curb distance is 33' wide.
- There is a sidewalk on both sides of the road but no bicycle facilities.

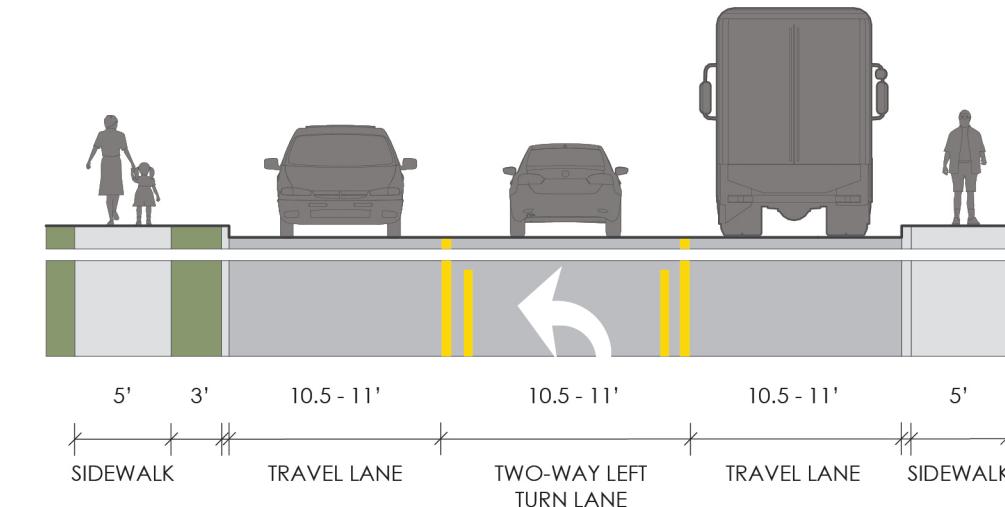
Proposed Typical Section

- The proposed typical section creates a lane diet with two travel lanes and a two-way center left turn lane.
- The existing curb-to-curb width is widened to approximately 38'.
- A multiuse path is proposed on the eastern side of the roadway, and a sidewalk is proposed on the western side of the roadway.

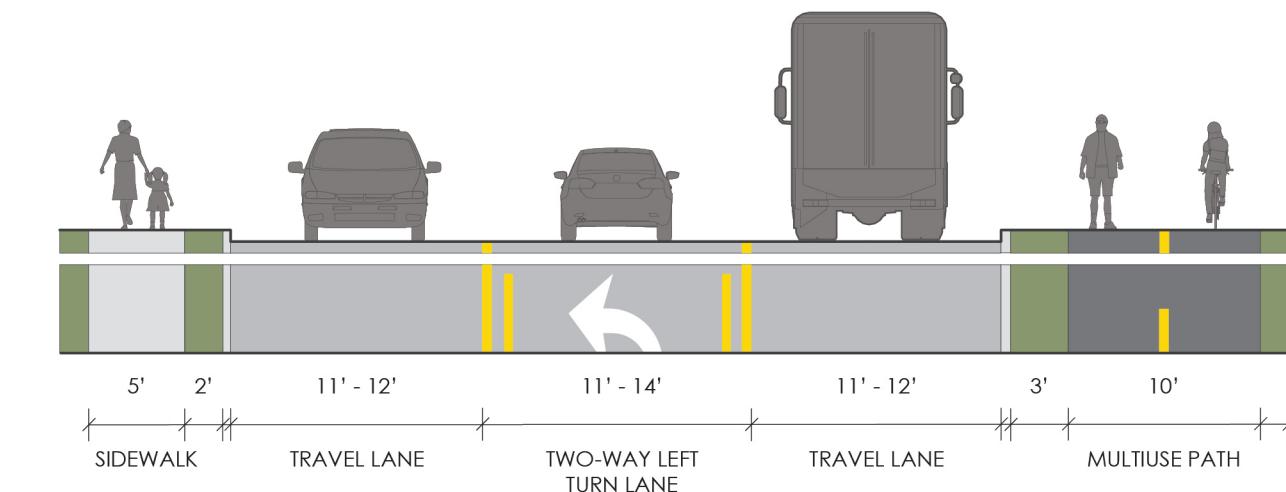
Schematic Design



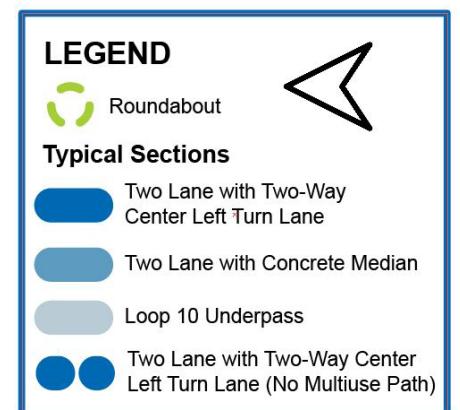
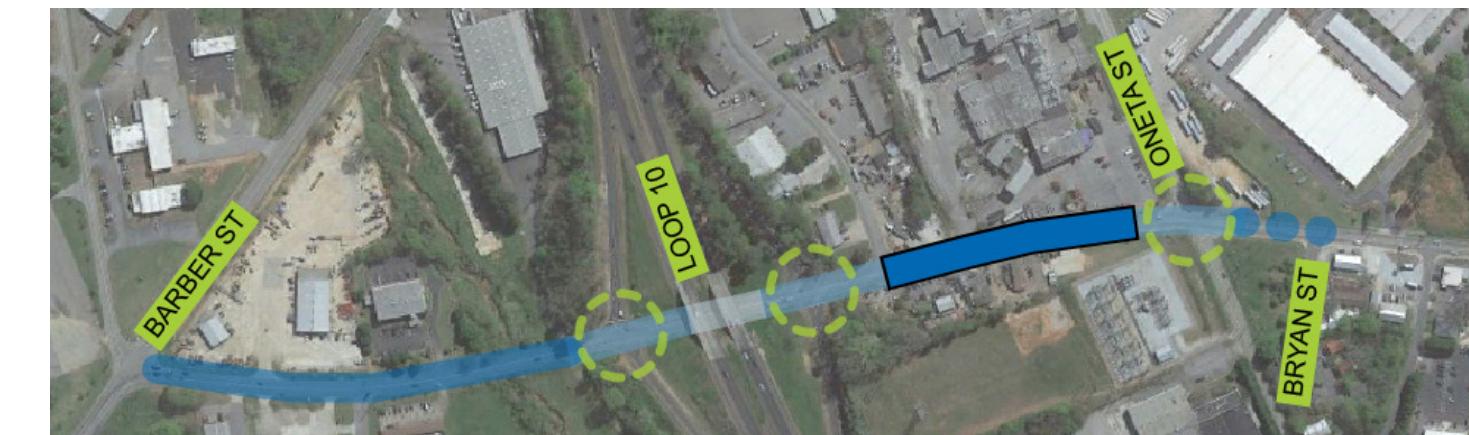
TYPICAL SECTION EXISTING



TYPICAL SECTION PROPOSED



CONTEXT MAP



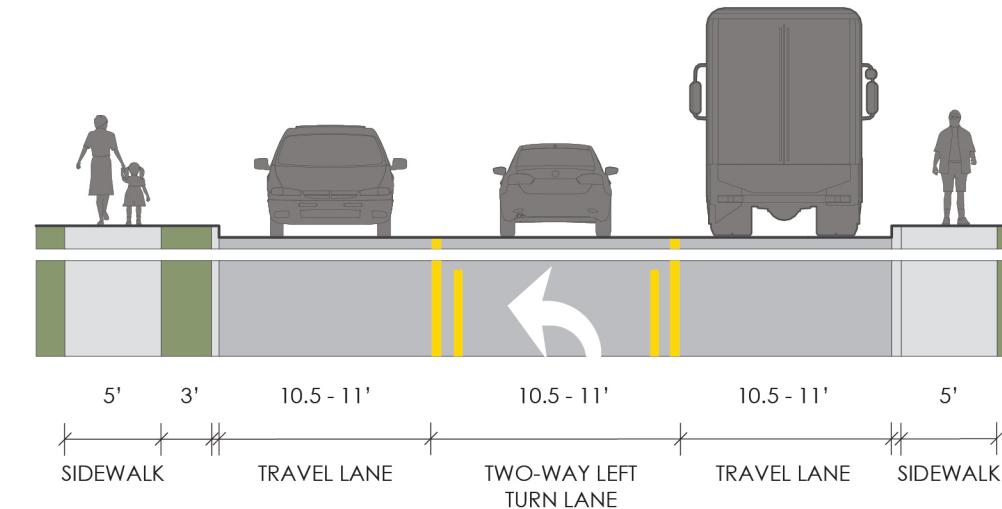
NORTH CHASE STREET

ONETA STREET APPROACH

Existing Conditions

- The existing typical section is two 10.5' – 11' wide travel lanes, one in each direction, with a 10.5' – 11' wide two-way center left turn lane.
- The existing curb-to-curb distance is 33' wide.
- There is a sidewalk on both sides of the road but no bicycle facilities.

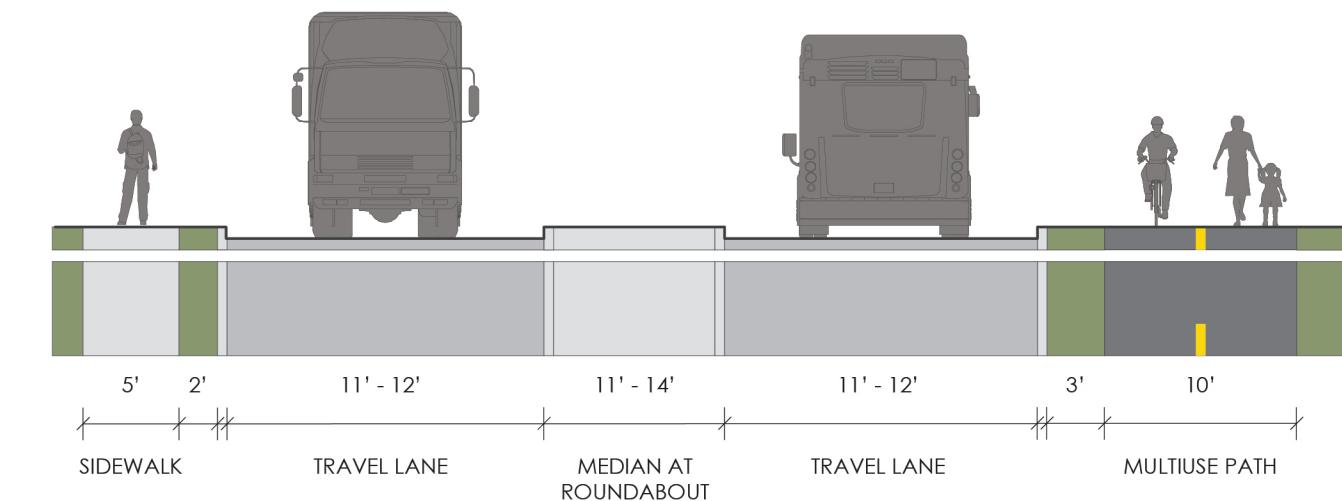
TYPICAL SECTION EXISTING



Proposed Typical Section

- The proposed typical maintains the two lanes with a center left turn lane condition.
- The lanes are widened to a minimum 11' wide.
- A multiuse path is proposed on the eastern side of the roadway, and a sidewalk is maintained on the western side of the roadway.
- **The multiuse path ends just south of the Oneta Street roundabout** and the sidewalk ties into the existing sidewalk to Bryan Street.

TYPICAL SECTION PROPOSED



Schematic Design



CONTEXT MAP

